



# Historical Resources Survey Report

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Capital Express Central – Intensive-level Survey:

Walker Brothers Warehouse, 807 East 4<sup>th</sup> Street

**Project Name:** Interstate Highway (I-) 35 Capital Express Central Project

**Project Limits:** U.S. Highway (US) 290 East to US 290 West/State Highway (SH) 71

**District(s):** Austin

**County(s):** Travis

**CSJ Number(s):** 0015-13-388

**Principal Investigators:** Rick Mitchell and Emily Pettis, Mead & Hunt, Inc.

**Report Completion Date:** May 2022

This historical resources survey report is produced for the purposes of meeting requirements under Section 106 of the National Historic Preservation Act, the Antiquities Code of Texas, and other cultural resource legislation related to environmental clearance as applicable.

## Abstract

The proposed Interstate Highway (I-) 35 Capital Express Central Project is to improve an approximately 8-mile segment of I-35 from U.S. Highway (US) 290 East to US 290 West/State Highway 71. The project Area of Potential Effects (APE) includes a portion of the Walker Brothers Warehouse at 807 East 4<sup>th</sup> Street, which was recommended as individually eligible for the National Register of Historic Places (NRHP) as part of the 2016 East Austin Survey conducted by Hardy-Heck-Moore, Inc. Based on these previous findings, the Texas Department of Transportation (TxDOT) concluded that intensive-level work would be required to provide a definitive NRHP evaluation for the property. A description of the overall project area and detailed survey results are included in the draft reconnaissance *Historic Resources Survey Report* dated March 2022.

On behalf of the TxDOT Environmental Affairs Division, Mead & Hunt, Inc. performed an intensive survey in January 2022 to provide a definitive evaluation of NRHP eligibility for the Walker Brothers Warehouse. The intensive survey included extensive research, fieldwork, and comparative analysis.

Based on the findings of the intensive survey, the Walker Brothers Warehouse is recommended eligible for listing in the NRHP under Criterion A in the area of Industry and Criterion C in the area of Architecture, both at the local level of significance. NRHP-eligible boundaries are limited to the building footprint on the north part of the property parcel. The south part of the property parcel, an open storage area surrounded by non-historic fencing, was not historically associated with the warehouse building. Proposed build alternatives would move I-35 main lanes closer to the NRHP-eligible building. However, vehicular lanes would be depressed below grade in both build alternatives, resulting in reduced noise and visual impacts compared to the existing freeway configuration.

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## Project Identification

- **Report Completion Date:** 05/06/2022
- **Date(s) of Fieldwork:** 01/14/2022
- **Survey Type:**  Windshield  Reconnaissance  Intensive
- **Report Version:**  Draft  Final
- **Regulatory Jurisdiction:**  Federal  State
- **TxDOT Contract Number:** WA57008SH004
- **District or Districts:** Austin
- **County or Counties:** Travis
- **Highway or Facility:** Interstate Highway (I-) 35
- **Project Limits:**
  - **From:** U.S. Highway (US) 290 East
  - **To:** US 290 West/State Highway (SH) 71
- **Main CSJ Number** 0015-13-388
- **Report Author(s):** Katherine Oldberg and Rick Mitchell; Mead & Hunt, Inc. (Mead & Hunt)
- **Principal Investigators:** Rick Mitchell and Emily Pettis, Mead & Hunt
- **List of Preparers:**

Rick Mitchell – Principal Investigator, directed fieldwork and research activities, performed quality control for intensive survey products, reviewed National Register of Historic Places (NRHP) evaluation, prepared effect recommendation.

Emily Pettis – Principal Investigator, performed quality control for intensive survey products, reviewed NRHP evaluation and effect recommendation.

Katherine Oldberg – primary author of intensive survey report, prepared NRHP recommendation.

Lauren Kelly – conducted fieldwork and research, assisted in intensive survey report preparation.

Caroline Bruchman – prepared field survey maps, survey report maps, and GIS deliverables.

## Area of Potential Effects (APE)

- Existing ROW
- 150' from Proposed ROW and Easements
- 300' from Proposed ROW and Easements
- Custom: Former Walker Brothers Warehouse, 807 East 4<sup>th</sup> Street, Travis Central Appraisal District (TCAD) parcel 191640. (Note: The APE for the I-35 Capital Express Central Project reconnaissance Historic Resources Survey Report [HRSR] extends 150 feet beyond the project's Environmental Study Area.)

▪ **Historic-Age Survey Cut-Off Date:** 1980

▪ **Study Area** For the I-35 Capital Express Central Project as a whole, the historic resources study area extends 1,300 feet beyond proposed new right-of-way (ROW) and easements. Results of the overall study area analysis are available in the project's Historic Resources Research Design and reconnaissance-level HRSR.

For purposes of this intensive survey, the historic resources study area is limited to the Walker Brothers Warehouse parcel.

## Section 106 Consulting Parties

▪ **Public Involvement Outreach Efforts:**

The proposed project includes ongoing focused Section 106 of the National Historic Preservation Act of 1966, as amended (Section 106) public involvement outreach, as well as incorporation of historic resources as part of the National Environmental Policy Act (NEPA) public involvement process. The Texas Department of Transportation (TxDOT) held a virtual Section 106 consulting parties meeting on October 6, 2021, to provide an overview of the project, cultural resources management as part of TxDOT's project development process, consulting party opportunities and roles in the Section 106 process, and upcoming historic resources survey tasks and schedule. TxDOT will involve consulting parties and other interested parties to provide input on potentially historic resources and historic districts in the project APE and study area. TxDOT will involve consulting parties throughout the Section 106 process,

including review of the findings of historic resources investigations and potential mitigation activities. Additional Information on Section 106-focused public involvement efforts for the I-35 Capital Express Central Project is described in the March 2022 reconnaissance-level HRSR.

▪ **Identification of Section 106 Consulting Parties:**

The following groups and individuals have been identified as potential consulting parties to date:

- Texas Historical Commission
- Travis County Historical Commission
- Preservation Austin
- Preservation Texas
- City of Austin Historic Landmark Commission/Historic Preservation Office
- Six Square Cultural District
- Black Austin Coalition
- Austin Area Urban League
- Black Leaders Collective

▪ **Section 106 Review Efforts:**

As noted above, TxDOT held a Section 106 consulting party meeting on October 6, 2021, to provide an overview of the project, cultural resources management as part of TxDOT's project development process, consulting party opportunities and roles in the Section 106 process, and upcoming historic resources survey tasks and schedule. On December 6, 2021, TxDOT updated consulting parties on the in-progress cultural resources investigations via email. TxDOT provided another update and outlined upcoming steps in the Section 106 process via email on February 2, 2022. In this email, TxDOT reminded consulting parties of the opportunity to provide input on potentially historic resources in the project APE. Mead & Hunt contacted several consulting parties to request additional information in the course of developing the draft HRSR. These included:

- City of Austin Historic Landmark Commission/Historic Preservation Office
- City of Austin Parks and Recreation Department, Historic Preservation and Tourism
- Preservation Austin
- Texas Historical Commission

TxDOT will continue to involve consulting parties throughout the Section 106 process, including review of the findings of historic resources investigations and potential mitigation activities. TxDOT will provide the draft reconnaissance and intensive-level HRSRs to all consulting parties for review and comment.

▪ **Summary of Consulting Parties Comments:**

There have been no consulting party comments regarding the Walker Brothers Warehouse to date.

## Stakeholders

- **Stakeholder Outreach Efforts:**

In addition to the Section 106 consulting parties, anticipated project stakeholders may include property owners, residents, and business owners in and near the project APE; neighborhood associations and planning contact teams; other neighborhood and community groups; and elected officials. The reconnaissance-level HRSR provides additional information on stakeholder outreach for the overall I-35 Capital Express Central Project.

As part of the intensive survey, Mead & Hunt identified the following additional potential stakeholders: Anchor Equities, Ltd., property owner.

- **Identification of Stakeholder Parties:**

Anchor Equities, Ltd.  
3839 Bee Cave Road Suite B-203  
Austin, TX 78746

- **Summary of Stakeholder Comments:**

Mead & Hunt and TxDOT have not contacted Anchor Equities, Ltd. at the time of draft report submittal.

## Project Setting/Study Area

- **Study Area**

The overall historic resources study area for the I-35 Capital Express Central Project extends 1,300 feet beyond maximum proposed ROW. For the purposes of this intensive survey, the study area is confined to the parcel boundary of the Walker Brothers Warehouse. Parcel data obtained through TCAD shows that the property comprises TCAD parcel 191640, which contains 0.6662 acres.

- **Previously Evaluated Historic Resources**

This property was evaluated by Hardy-Heck-Moore, Inc. (HHM, Inc.) as part of the 2016 East Austin Survey (HHM ID 3398) and recommended as individually eligible for the NRHP.

- **Previously Designated Historic Properties**

There are no previously designated historic properties within the intensive survey area.

- **Previously Designated Historic Districts**

There are no previously designated historic districts on this property.

- **Historic Land Use**

Sanborn maps show that, prior to construction of the Walker Brothers Warehouse, a shed associated with a lumber yard was located on the lot. By 1873 industrial development began lining the Houston & Texas Central (H&TC) Railroad corridor, which ran just north of the property. Shortly thereafter, the Walker Brothers Warehouse was constructed. The property and surrounding lots remained a mix of industrial and residential through the late twentieth century. The Walker Brothers Warehouse was used for warehouse storage through the 2010s.

- **Current Land Use and Environment**

The current owner purchased the property in 2014 and converted the space into a hostel, business, and an event space/music venue, known as “Native Hostel.” As of early 2022 the hostel had been permanently closed.

The I-35 northbound frontage road borders the property to the west. The Capital Metro Red Line rail tracks and large recent commercial and mixed-use developments border it to the north. The Austin Metal & Iron Company (Resource 403A-C, 300 Medina Street) and historic-age residential properties border it to the east and a courtyard and parking lot borders it to the south.

- **Historic Period(s) and Property Types**

The subject property is a c.1880 warehouse that initially served an industrial function and was converted to commercial and office space in the late twentieth century. In the early twenty-first century it was converted into a hostel, commercial space, and music venue.

- **Integrity of Historic Setting**

The integrity of the historic industrial setting was diminished by the construction of I-35 to the west in the early 1960s, which cut through the western portion of the block where additional industrial and residential properties had been located. Recent commercial and mixed-use development to the north continues to compromise the integrity of the historic setting. Detailed integrity information is provided in the NRHP Eligibility Recommendations section below.

## **Survey Methods**

- **Methodological Description**

The intensive survey is in compliance with TxDOT’s latest Environmental Toolkit Standards and Historical Studies guidance. A reconnaissance-level survey has been performed for the full project corridor between US 290 East and US 290 West/SH 71. This intensive-level investigation focused on clarifying the NRHP eligibility of the Walker Brothers Warehouse at 807 East 4<sup>th</sup> Street in East Austin. A map is provided in Appendix D. The historic-age resource was identified using available historic maps, atlases, and aerials in combination with field observations. The

property was evaluated for potential NRHP eligibility applying appropriate regional and thematic contexts. The historic context was developed using primary and secondary source material.

On behalf of TxDOT ENV, Mead & Hunt performed an intensive survey of the Walker Brothers Warehouse in January 2022. Field survey for the property included detailed photography and fieldnotes recorded electronically on tablets. Mead & Hunt did not access the interior of the building. Survey photographs and field data were used to produce inventory forms for the historic-age resource (see Appendix C).

- **Comments on Methods**

Investigations include one non-archaeological cultural resources intensive survey, including photographic documentation and research. Findings of the intensive survey are compiled into this HRSR completed in accordance with TxDOT's *Documentation Standard: Historical Resources Survey Report*.

## Survey Results

- **Project Area Description**

Two alternatives are currently under consideration for the proposed improvements: Alternative 2 and Modified Alternative 3. Under both alternatives, proposed improvements include removing the existing I-35 upper decks, lowering the roadway, and adding two non-tolled high-occupancy-vehicle managed lanes in each direction from US 290 East to SH 71/Ben White Boulevard. Both alternatives would reconstruct frontage roads, ramps, intersections, and east-west cross-street bridges. Shared-use (pedestrian and bicycle) paths would be added along the corridor. Under Alternative 2, additional flyovers would be constructed at the I-35 and US 290 East interchange. Under both alternatives, additional ROW acquisition would be required intermittently along areas throughout the corridor. In most areas, the proposed new ROW would not extend past the first tier of parcels adjacent to the existing I-35 ROW. Design files for both alternatives are available on the project website ([www.my35capex.com](http://www.my35capex.com)). Parcels adjacent to the I-35 ROW include a mix of historic-age and non-historic-age commercial, institutional, or high-density multi-family residential resources. Several parks, cemeteries, and sports and entertainment stadiums/arenas are also present. Beyond the first tier of parcels, the APE surveyed for the project's reconnaissance HRSR includes a wider mix of resources, including established single-family residential neighborhoods, a wide range of commercial properties, governmental buildings, and educational properties.

This intensive survey focuses only on the Walker Brothers Warehouse, located just east of downtown Austin in East Austin. As noted above, the Walker Brothers Warehouse encompasses an area bounded by East 3<sup>rd</sup> Street to the south, Brushy Street to the east, East 4<sup>th</sup> Street and the Capital Metro Red Line rail tracks to the north, and the I-35 northbound frontage road to the west. It is currently occupied by Native Hostel (no longer in operation) and other commercial

tenants. The immediate surrounding area features large-scale infill properties including commercial, mixed-use, and residential development.

## ▪ Literature Review

Mead & Hunt consulted a range of primary and secondary sources to develop the historic context and reevaluate the subject property for NRHP eligibility. Below is a general description of sources used and how each informed the intensive survey. Individual resources are also identified in the References Cited section.

The following secondary sources provided both general background information on the study area and property-specific information for the historic context:

- Entries from the *Handbook of Texas Online*.
  
- Historic contexts on the City of Austin and I-35 previously prepared by HHM, Inc.:
  - City of Austin Historic Resources Study, 2016
  - Interstate Highway 35 Corridor Historic Resources Investigations, 2004

The following historic mapping and aerial imagery sources obtained online were used to trace development patterns and land uses in the study area:

- Historic and current aerial imagery obtained through [Historicaerials.com](http://Historicaerials.com), Google Earth, and U.S. Geological Survey (USGS) Earth Explorer.
  
- USGS topographic maps of Austin available through USGS TopoView.
  
- Current property parcel data available from TCAD.
  
- Historic Sanborn maps obtained through the University of Texas at Austin, Perry-Castañeda Library Map Collection and the Library of Congress.
  
- Bird's-eye maps from the Perry-Castañeda Library Map Collection and Texas Historic Map Overlay.

Property-specific primary source materials were obtained online and at the Austin History Center. These materials were used along with other sources to develop a historic overview of the Walker Brothers Warehouse. These sources included the following:

- Historic photos of the Walker Brothers Warehouse available at the Austin History Center.
  
- Building permit and inspection records for the Walker Brothers Warehouse available at the Austin History Center.

- Issues of the *Austin American-Statesman* and *Austin American* available through Newspapers.com and on the newspaper’s own website.
- Recent local news articles on investors Michael Dickson, Antonio Madrid, and Will Steakley and Native Hostel, as well as the website for Native Hostel.
- Historic photos from TxDOT Photo Library, City of Austin directories, and U.S. Census records 1850-1910.

## Historical Context Statement

### The Railroad and Industrial Development in Austin’s East Outlots

The history of the construction and use of the Walker Brothers Warehouse is tied to the presence of the railroad tracks running east-west through East Austin and downtown Austin. The H&TC Railroad was completed through Austin in December 1871 as the city’s first railroad. By 1873 the H&TC continued north of Austin through Dallas to reach Denison at the Oklahoma border, where it connected with the Missouri, Kansas, and Texas (MK&T) Railroad, providing a rail connection to St. Louis and the eastern United States and thereby enhancing regional and interstate commerce and trade.<sup>1</sup> In 1883 Southern Pacific bought the H&TC Railroad and it became a subsidiary of that line.<sup>2</sup>

The arrival of the H&TC initiated an era of industrial development in Austin along East and West 4<sup>th</sup> and 5<sup>th</sup> Streets, which ran to the south and north of the railroad corridor, respectively (see Figure 1). Warehouses and other industrial and agricultural processing buildings featured rail sidings that provided direct access to the tracks. The area east of East Avenue extending several blocks to the north and south of the railroad was known as the east “Outlots.” The 1885 Sanborn map shows the two-story stone Walker Brothers Warehouse (vacant) across from the H&TC and lumber yards, as well as a feed store, dwellings, and a harness shop to the west, and a general store, cotton warehouse, two saloons, and a cotton gin to the east (see Figures 2 and 3).<sup>3</sup> By 1890 the area became home to a diverse population that included immigrants and Black freedmen (see Figure 4).<sup>4</sup>

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<sup>1</sup> George C. Werner, “Houston and Texas Central Railway,” *Handbook of Texas Online*, accessed January 27, 2022, <https://www.tshaonline.org/handbook/entries/houston-and-texas-central-railway>.

<sup>2</sup> Michael Barnes, “Austin’s First Railroad Altered the City Forever,” *Austin American-Statesman*, May 23, 2019, <https://www.austin360.com/story/news/history/2019/05/23/on-christmas-day-1871-austins-first-railroad-altered-city-forever/5079567007/>.

<sup>3</sup> “Austin, Texas, June 1885,” 1:600 (New York: Sanborn Map & Publishing Company, 1885), Sheet 7, University of Texas at Austin, Perry-Castañeda Library Map Collection.

<sup>4</sup> HHM, Inc., *City of Austin Historic Resources Survey: Final Report, Volume I* (Prepared for the City of Austin, October 2016), 16.

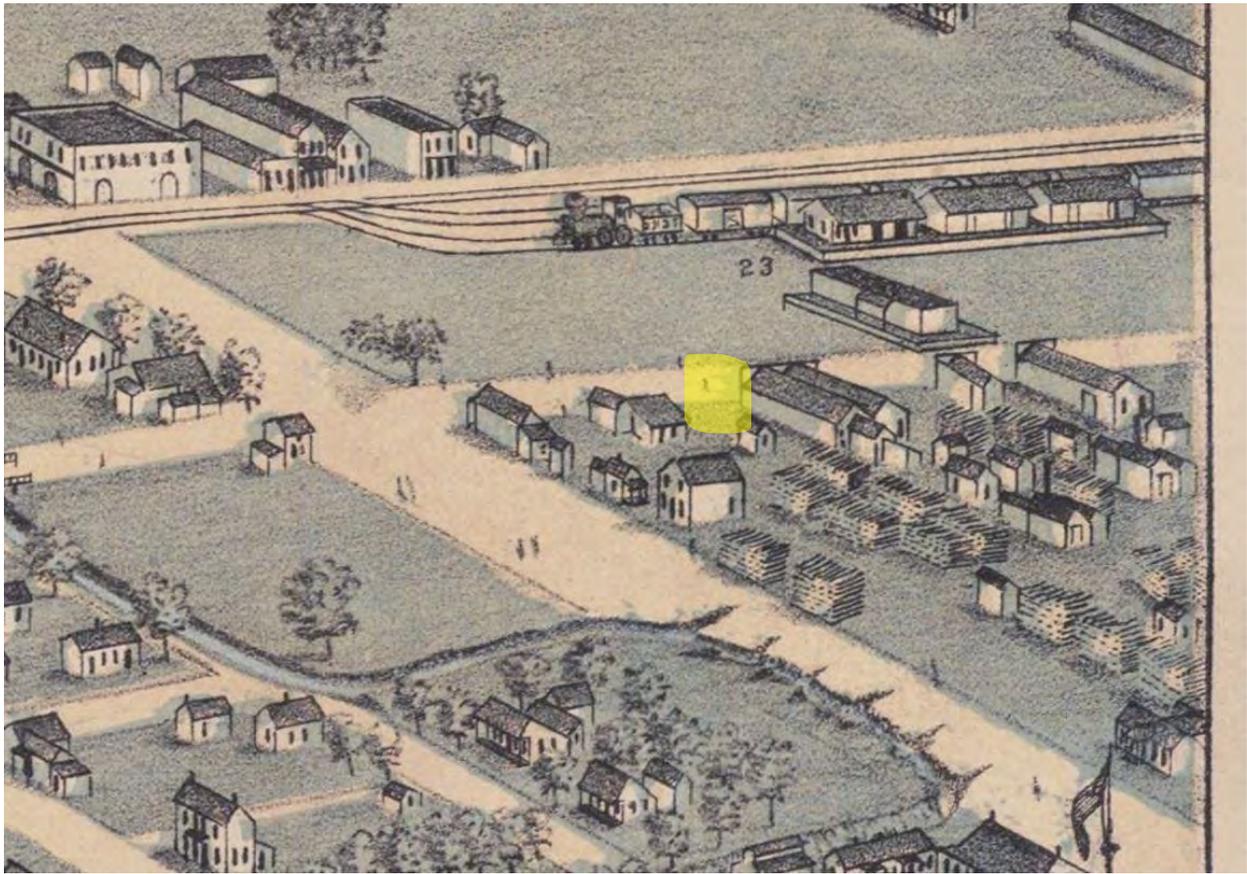


Figure 1. 1873 bird's-eye view of Austin with the future location of the Walker Brothers Warehouse highlighted. Note the industrial development along the railroad, the freight depots, and the lumber yard that dominates the subject block.<sup>5</sup>

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<sup>5</sup> Augustus Koch, "Bird's Eye View of the City of Austin, Travis County, Texas, 1873" (Madison, Wis.: J. J. Stoner, 1873), Perry-Castaneda Library at the University of Texas at Austin.

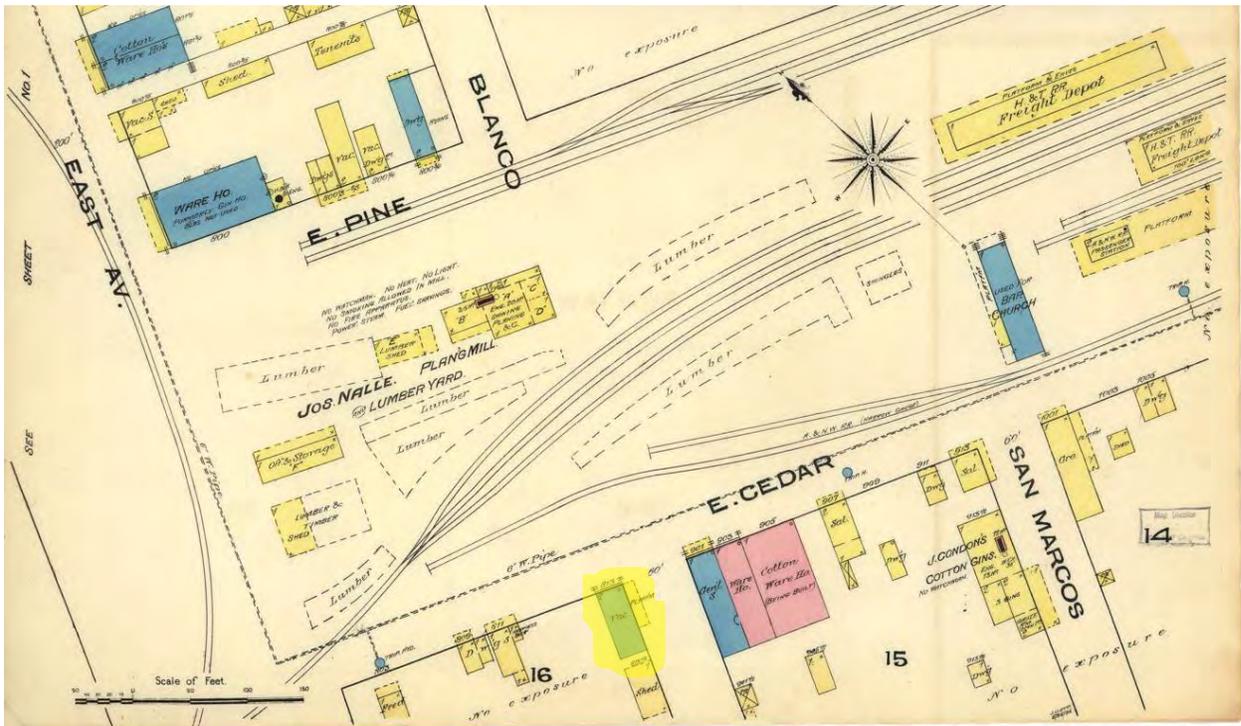


Figure 2. 1885 Sanborn map showing industrial development along the H&TC Railroad. The location of the Walker Brothers Warehouse is highlighted in bright yellow.<sup>6</sup>

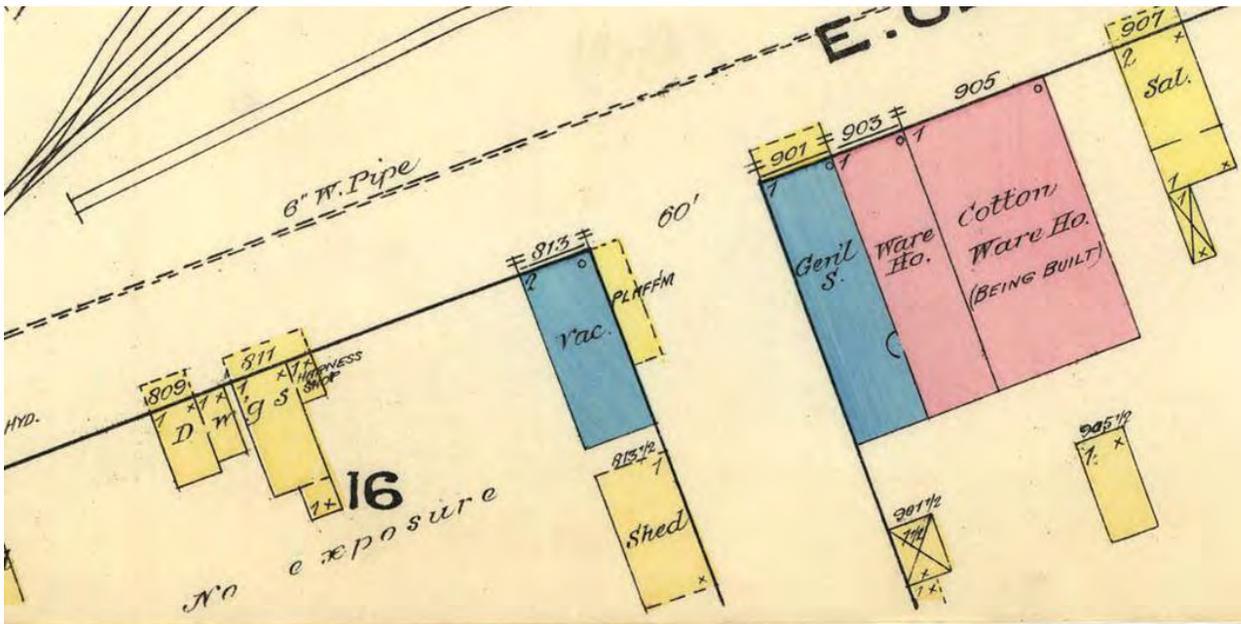


Figure 3. Zoomed-in look at the location of the Walker Brothers Warehouse and its immediate surroundings on the 1885 Sanborn map. The Walker Brothers Warehouse is shown as 813 East Cedar Street.<sup>7</sup>

<sup>6</sup> "Austin, Texas, June 1885," Sheet 7.

<sup>7</sup> "Austin, Texas, June 1885," Sheet 7.



Figure 4. 1890 bird's-eye view of Austin with the location of the Walker Brothers Warehouse highlighted. Note the industrial development along the railroad and tenement houses on the subject block.<sup>8</sup>

A historic context of Austin developed by HHM, Inc. for the City of Austin in 2016 described the considerable growth along this corridor in the early twentieth century:

The 1900s' first quarter saw rapid expansion of the east Outlots with development of many areas previously unplatted to meet the continuing demand....The year 1904...marked the arrival of the Missouri, Kansas and Texas Railroad (MK&T or "Katy") extension from Granger to Austin. Completed on June 15, 1904, the line tied into the existing track of the H&TC at the edge of East Austin. The MK&T used the tracks of the H&TC from the MK&T depot grounds in East Austin to the H&TC passenger depot located downtown for passenger service.<sup>9</sup>

The area saw additional rail traffic with the new MK&T line and the surrounding development continued well into the mid-twentieth century. The 1935 Sanborn map with 1962 updates shows considerable industrial growth along the railroad east of East Avenue, in the vicinity of the Walker Brothers Warehouse. The footprint of the subject building today matches the

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<sup>8</sup> Augustus Koch, "Partial View of Austin, Texas" (Kansas City, Mo.: Inter-State Publishing Company, 1890), Perry-Castaneda Library at the University of Texas at Austin, [https://maps.lib.utexas.edu/maps/historical/austin-1890-birdseyeaustin\\_01.jpg](https://maps.lib.utexas.edu/maps/historical/austin-1890-birdseyeaustin_01.jpg).

<sup>9</sup> HHM, Inc., *City of Austin Historic Resources Survey: Final Report, Volume I*, 36–38.

building depicted on this map, including the c.1935 and c.1953 additions (see Figures 5 and 6).<sup>10</sup>

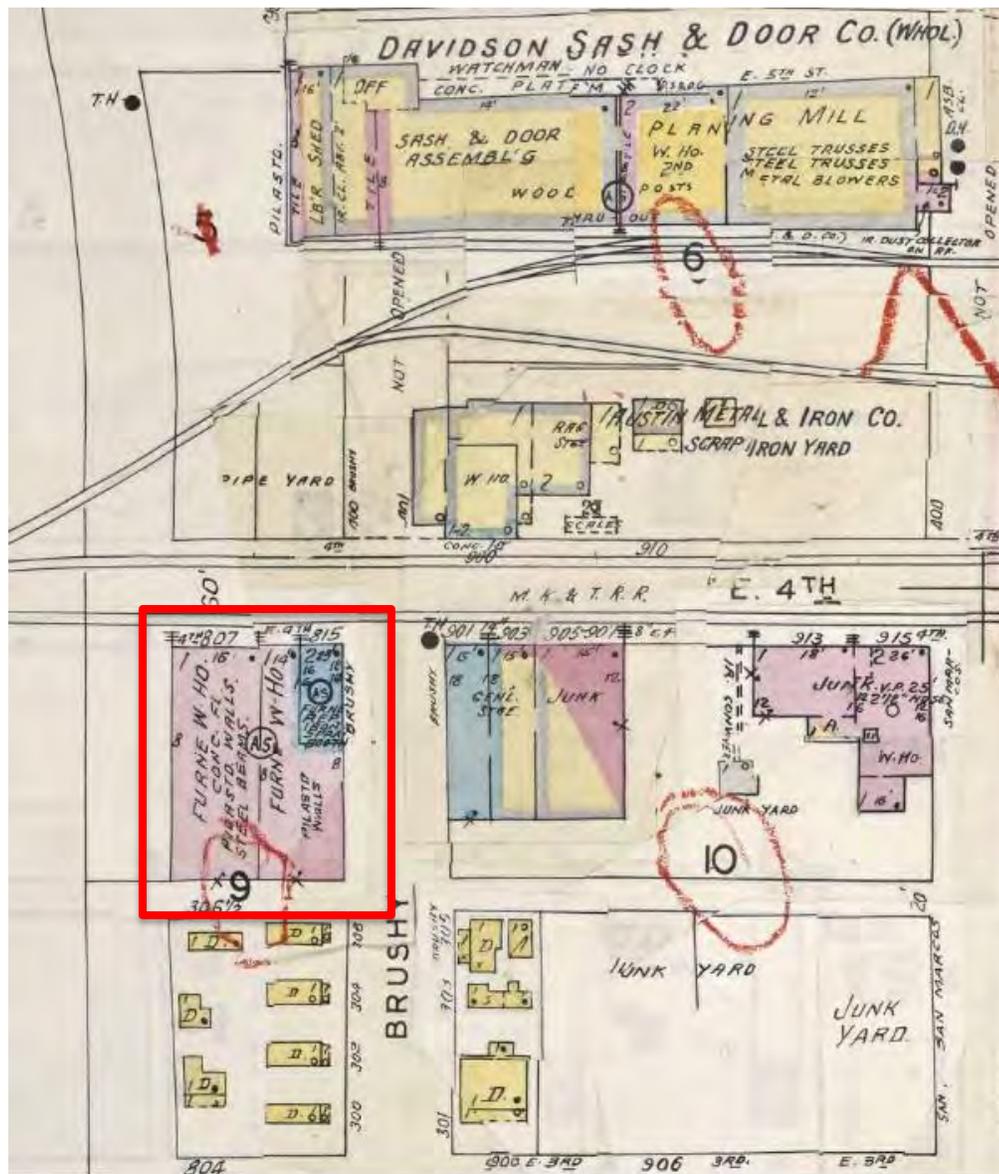


Figure 5. 1935 Sanborn map with 1962 updates showing industrial development along the rail corridor between East 4<sup>th</sup> and East 5<sup>th</sup> Streets. The Walker Brothers Warehouse is shown in the red square with the c.1935 addition on the side (west) and rear (south) elevations, and the c.1953 addition shown as 807 East 4<sup>th</sup> Street. Note that I-35 has replaced the dwellings previously located to the west of the warehouse.<sup>11</sup>

<sup>10</sup> "Austin, Texas, 1935 (Revised 1962) Vol. 2.," 1:600 (New York, 1962), Sheet 211, Digital Sanborn Maps, 1867-1970, ProQuest.

<sup>11</sup> "Austin, Texas, 1935 (Revised 1962) Vol. 2.," Sheet 211.

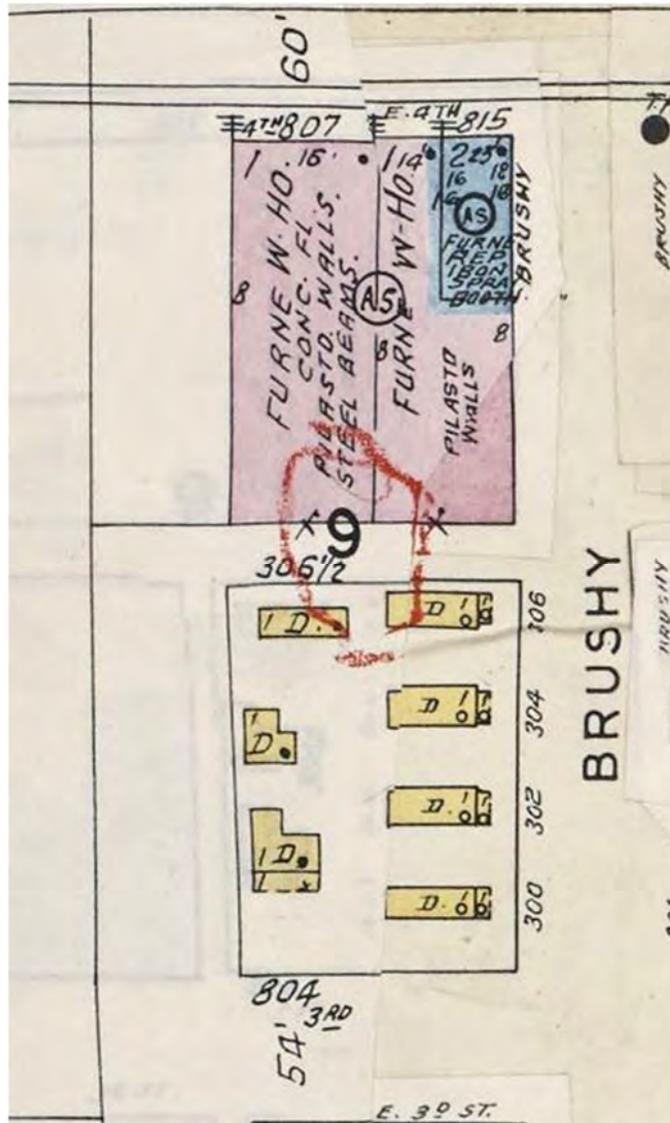


Figure 6. Zoomed-in look at the Walker Brothers Warehouse on the 1935 Sanborn map with 1962 updates. Note the dwellings located to the south of the warehouse. The dwellings were demolished in the 1980s and that area, which is now part of the legal parcel, is now comprised of a parking lot, courtyard, and ad-hoc storage."<sup>12</sup>

### Walker Brothers Warehouse, 1880-1890

The Walker Brothers Warehouse first appears in the 1881-1882 *Morrison & Fourmy's General Directory of the City of Austin*.<sup>13</sup> Multiple entries include "Walker Bros." under William B. and James J. Walker located on Cedar Street (now East 4<sup>th</sup> Street) between East Avenue (now I-35) and Blanco Street (now Brushy Street). One of the entries advertised the establishment sold

<sup>12</sup> "Austin, Texas, 1935 (Revised 1962) Vol. 2.," Sheet 211.

<sup>13</sup> *Morrison & Fourmy's General Directory of the City of Austin* (Austin, Texas: Morrison & Fourmy, 1881), 166, 167, 186, 188, 189, <https://texashistory.unt.edu/ark:/67531/metaph39151/>.

“groceries, dry goods, hardware, cotton, wool, and hides.”<sup>14</sup> An earlier city directory from 1877-1878 includes entries for the general merchants, saloons, a carpenter, and druggist on Cedar Avenue east of the subject property between Blanco Street and San Marcos Street (see Figures 2 and 3). The 1877-1878 directory does not mention a cold storage building or business between East Avenue (now I-35) and Blanco Street. Residences are listed on Cedar Avenue between East Avenue and Blanco Street. It is likely the subject two-story stone warehouse was built between 1877 and 1881 on vacant land or replaced demolished dwellings.<sup>15</sup>

William Bailey (W.B.) Walker was born in Jackson, Mississippi, on September 8, 1840, and his younger brother, James J. Walker, was born in 1848 in New Madrid County, Missouri.<sup>16</sup> In 1860, one year before the start of the Civil War, W.B. was 20 and lived with his parents in Lavaca County, Texas, along with his brothers and sisters.<sup>17</sup>

Following the Civil War, W.B. moved to Austin in 1871. The Austin city directory from 1872, as well as an advertisement placed in the November 6, 1872, edition of the *Tri-Weekly State Gazette*, list Walker as one of the proprietors of P. Thompson & Co., a merchant company based out of a building “near the freight depot” (see Figure 7).<sup>18</sup> It is likely Thompson and Walker operated their business out of a building on East Cedar Avenue near the H&TC Railroad freight depots and east of the future location of the Walker Brothers Warehouse (see Figures 1-4).<sup>19</sup> Their advertisement notes trading in products like cotton and wool but does not discuss selling groceries.

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<sup>14</sup> Morrison & Fourmy's General Directory of the City of Austin, 1881, 167.

<sup>15</sup> Mooney & Morrison's General Directory of the City of Austin (Houston, Texas: Mooney & Morrison, 1877), <https://texashistory.unt.edu/ark:/67531/metaph46838/>; “Austin, Texas, June 1885,” 7.

<sup>16</sup> “William B. Walker, Merchant Here for Many Years, Is Dead,” *The Austin American*, July 25, 1917, <https://www.newspapers.com/image/384497879/?terms=william%20b%20walker&match=1>; United States of America, Bureau of the Census, *Seventh Census of the United States, 1850* (Washington, D.C.: National Archives and Records Administration, 1850), 305, Ancestry.com; United States of America, Bureau of the Census, *The Eighth Census of the United States, 1860* (Washington D.C, 1860), 186, 187, <https://www.ancestry.com/discoveryui-content/view/35054676:7667>.

<sup>17</sup> United States of America, Bureau of the Census, *The Eighth Census of the United States, 1860*, 186.

<sup>18</sup> *Mercantile and General City Directory of Austin Texas – 1872-73* (Houston, Texas: Gray & Moore, 1872), 110, 126, <https://texashistory.unt.edu/ark:/67531/metaph38126/>; “Commission Merchants,” *Tri-Weekly State Gazette*, n.d., 4, <https://texashistory.unt.edu/ark:/67531/metaph181071/m1/4/>.

<sup>19</sup> “Austin, Texas, June 1885,” Sheet 7.

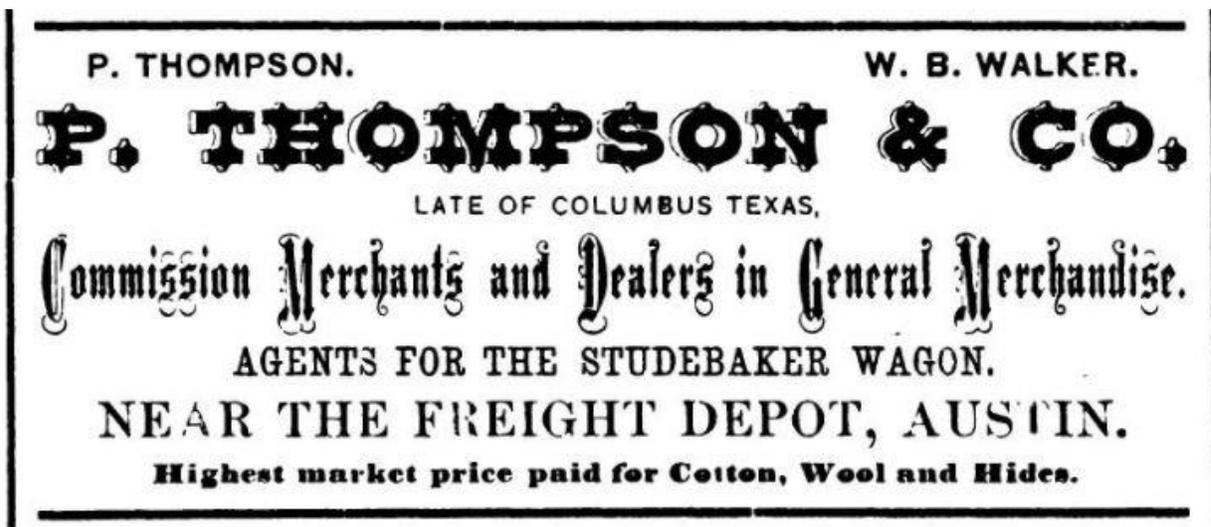


Figure 7. Advertisement for the P. Thompson & Co. in the 1872-1873 city directory of Austin, listing P. Thompson and W.B. Walker as proprietors.<sup>20</sup>

Sometime between 1872 and 1877 W.B. struck out on his own and opened W.B. Walker & Co. on Pecan Street (now East 6<sup>th</sup> Street) between Congress Avenue and Colorado Avenue, which sold groceries and "provisions."<sup>21</sup> The 1877 Austin city directory lists W. Bailey and James J. Walker as proprietors of W.B. Walker & Co., which was noted as one of almost 100 grocers in Austin at that time.<sup>22</sup> In the 1881 directory the company appears as the "Walker Bros. (W.B. and J.J. Walker)" with their store located at 205 Pecan Street, between Brazos and San Jacinto (extant), and their storage warehouse located at the subject property on Cedar Street between East Avenue and Blanco Street (now Brushy Street).<sup>23</sup>

City directories from 1883 to 1903 list W.B. Walker as a grocer with a retail store at 208-210 East 6<sup>th</sup> Avenue and wholesale grocery at 901-911 East 4<sup>th</sup> Street, immediately to the east of the subject property.<sup>24</sup> The city directories between 1883 and 1903 do not list James J., implying he left the business and Austin sometime between 1882 and 1883. According to city directories, newspaper ads in *The Austin American*, the 1910 census, and W.B.'s obituary, he later changed the name of his store to W.B. Walker & Sons to include his three sons: William Delaney, James Claire, and Thomas Bailey. W.B.'s obituary states he left the grocery business and started dabbling in real estate later in life. W.B. died in 1917 at the age of 77 and is buried

<sup>20</sup> *Mercantile and General City Directory of Austin Texas – 1872-73*, 126.

<sup>21</sup> *Mooney & Morrison's General Directory of the City of Austin*, 241.

<sup>22</sup> *Mooney & Morrison's General Directory of the City of Austin*, 219, 241.

<sup>23</sup> *Morrison & Fourmy's General Directory of the City of Austin*, 1881, 166, 167, 186, 188.

<sup>24</sup> *Morrison & Fourmy's General Directory of the City of Austin* (Galveston, Texas: Morrison & Fourmy, 1897), 337, <https://texashistory.unt.edu/ark:/67531/metaph39149/>.

at Oakwood Cemetery.<sup>25</sup> Review of local newspapers, archives, and online repositories provided little additional information about James J. Walker.

### **Walker Brothers Warehouse, 1890s-Present**

By 1883 W.B. Walker had moved his business to a different building east of the subject property. The 1885 Sanborn shows the two-story stone subject building as vacant (see Figures 2-3), and the 1894 Sanborn maps shows the building as “cold storage” (see Figure 8).<sup>26</sup> The city directory from 1897-1898 lists the San Antonio Brewing Association “City brewery” office and beer vaults at this location, with Philip L. Meyer as agent.<sup>27</sup> According to the 2016 evaluation by HHM, Inc. (HHM ID 3398), the San Antonio Brewing Association resided in the building through 1916.<sup>28</sup> Based on the size of the building and its cold storage function, it is likely the subject building served as a local distribution/storage location for the brewery.

The San Antonio Brewing Association was started in the 1880s by a group of Texans in San Antonio who bought the J.B. Boehloradsky Brewery (1881-1883). In 1886 the San Antonio Brewing Association started producing Pearl Beer. Under the leadership of Otto Koehler, the San Antonio Brewing Association became the largest brewery in Texas by 1916.<sup>29</sup> Koehler was considered one of the wealthiest men in the Southwest, with investments in bank stock, mines, and real estate, in addition to his involvement in the San Antonio Brewing Association. After Koehler's death in 1914, ownership of the warehouse transferred to his wife Emma, who also took over the brewery.<sup>30</sup>

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<sup>25</sup> “William B. Walker, Merchant Here for Many Years, Is Dead”; United States of America, Bureau of the Census, *The Thirteenth Census of the United States, 1910* (Washington D.C, 1910), [https://www.ancestry.com/discoveryui-content/view/28677197:7884?tid=&pid=&queryId=b33a1a305ea20d952bc5659099f3a3b6&\\_phsrc=vMS857&\\_phstart=successSource](https://www.ancestry.com/discoveryui-content/view/28677197:7884?tid=&pid=&queryId=b33a1a305ea20d952bc5659099f3a3b6&_phsrc=vMS857&_phstart=successSource).

<sup>26</sup> “Austin, Travis County, Texas, January 1894,” 1:600 (New York: Sanborn-Perris Map Company, 1894), Sheet 20, University of Texas at Austin, Perry-Castañeda Library Map Collection.

<sup>27</sup> *Morrison & Fourmy's General Directory of the City of Austin, 1897*, 220.

<sup>28</sup> HHM, Inc., *City of Austin Historic Resources Survey: Final Report, Volume IV* (Prepared for the City of Austin, October 2016), 44.

<sup>29</sup> Mike Hennech, “The Business of Brewing,” *Heritage Magazine*, 2005, 21, Portal to Texas History, <https://texashistory.unt.edu/ark:/67531/metaph45370/>.

<sup>30</sup> Hennech, “The Business of Brewing,” 21–22; Michael C. Hennech, “Pearl Brewing Company,” *Texas State Historical Association Handbook of Texas*, n.d., <https://www.tshaonline.org/handbook/entries/pearl-brewing-company>; Michael C. Hennech and Tracé Etienne-Gray, “Brewing Industry,” *Texas State Historical Association Handbook of Texas*, n.d., <https://www.tshaonline.org/handbook/entries/brewing-industry>. In 1952, SABA changed its name to Pearl Brewing Company. Today Pabst Brewing Company contracts with Miller Brewing Company to make Pearl beer.

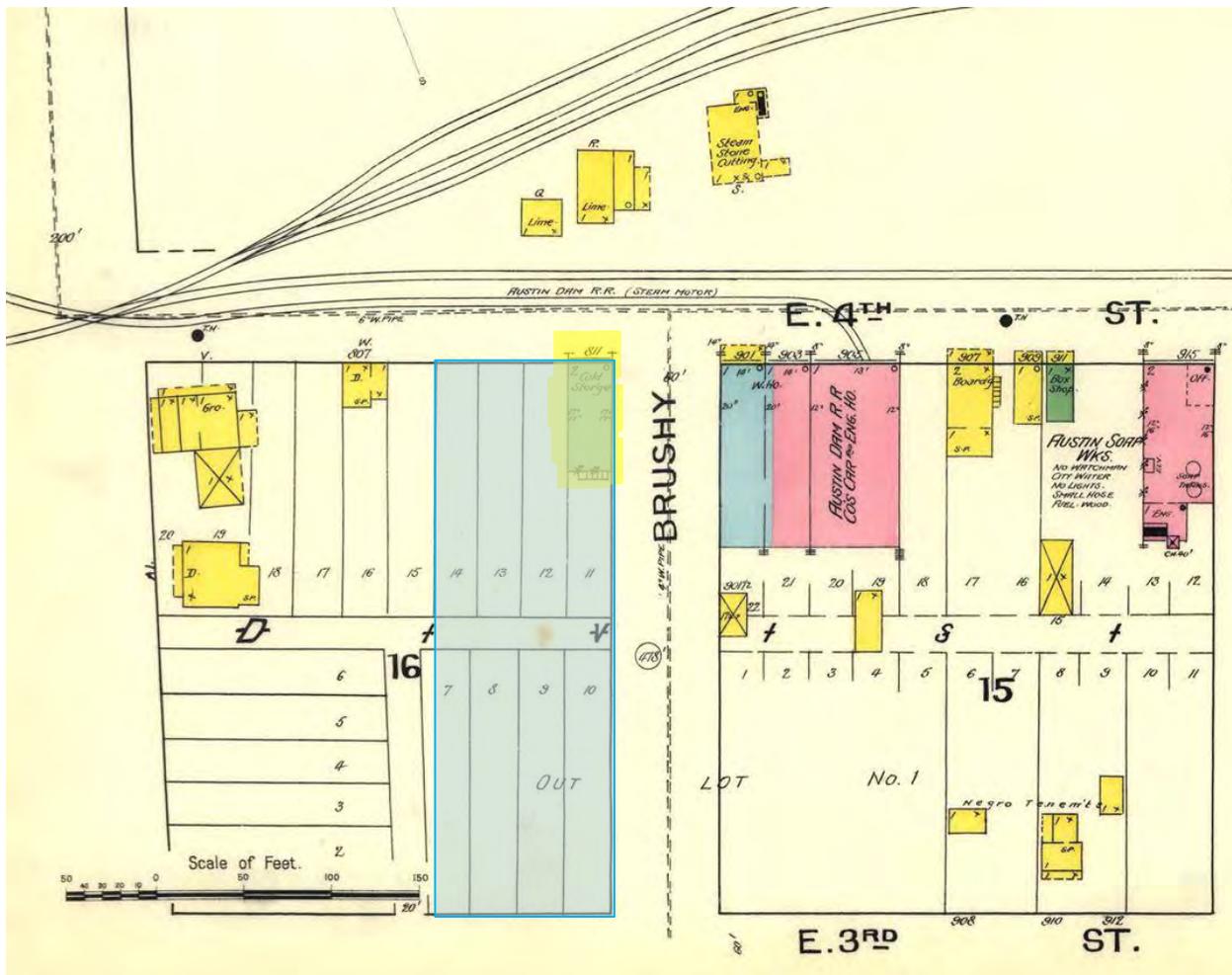


Figure 8. 1894 Sanborn map showing the Walker Brothers Warehouse as “cold storage,” as well as the lot numbers for block 16 of division O. The Walker Brothers Warehouse is highlighted in yellow and the current parcel boundary is highlighted in blue.<sup>31</sup>

Newspaper records of real estate transactions published in 1918 indicate the San Antonio Brewing Association sold multiple parcels to “Ella Koehler,” including lots 11 and 12, block 16 in division O, outlot 1 in January 1916.<sup>32</sup> This legal description matches the lot containing the original two-story stone warehouse building (lot 11), as well as the lot immediately to the west (lot 12) that would later include the c.1935 addition, the first of two additions to the side (west) elevation of the building (the addition c.1953 takes over lots 13 and 14) (see Figure 8). It is likely the newspaper misprinted Emma Koehler’s name, as deed records filed in 1918 confirm the transaction between San Antonio Brewing Association and Emma Koehler.<sup>33</sup>

<sup>31</sup> “Austin, Travis County, Texas, January 1894,” Sheet 20.

<sup>32</sup> “Real Estate Transfers,” *The Statesman*, March 5, 1918, Newspapers.com.

<sup>33</sup> “Deed, San Antonio Brewing Association to Emma Koehler,” March 4, 1918, Vol. 302, Page 429A, Register of Deeds Office, Travis County, Texas.

In 1918 Emma Koehler sold the property to Theo P. Meyer, son of San Antonio Brewing Association Austin agent Philip L. Meyer.<sup>34</sup> According to the HHM, Inc. 2016 survey of the property, Witcher Produce Company and Purina Feed Store were located in the warehouse building in 1920.<sup>35</sup> In 1922 Meyer sold the property to Charles J. Martin, who then deeded it to his business—C.J. Martin & Sons—in 1924.<sup>36</sup> C.J. Martin & Sons, a merchant supply company selling a variety of items including seeds, insecticide, veterinary supplies, and sanitary chemicals, operated its factory and warehouse out of the building until the 1950s.<sup>37</sup> Charles J. founded the business in 1883 and ran the company with his sons Charles Early and Leo Martin. From the 1920s through the 1940s their retail store was located at 112-118 East 4<sup>th</sup> Street (East 4<sup>th</sup> Street and Chicon Street, nonextant), west of the subject property (see Figure 9).<sup>38</sup>

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<sup>34</sup> “Deed, Emma Koehler to Theo P. Meyer,” March 15, 1918, Vol. 155, Page 307, Register of Deeds Office, Travis County, Texas; “Philip Louis Meyer - Find A Grave Memorial,” n.d., <https://www.findagrave.com/memorial/68515499/phillip-louis-meyer>.

<sup>35</sup> HHM, Inc., *City of Austin Historic Resources Survey: Final Report, Volume IV*, 44.

<sup>36</sup> “Deed, Theo P. Meyer to C.J. Martin,” October 14, 1922, Vol. 343, Page 615A, Register of Deeds Office, Travis County, Texas; “Deed, C.J. Martin to C.J. Martin & Sons,” February 26, 1924, Vol. 359, Page 319, Register of Deeds Office, Travis County, Texas.

<sup>37</sup> HHM, Inc., *City of Austin Historic Resources Survey: Final Report, Volume IV*, 44; “Certificate of Death for Charles J. Martin” (Texas State Board of Health, August 15, 1926), Ancestry.com, [https://www.ancestry.com/discoveryui-content/view/30255485:2272?tid=&pid=&queryId=dae45b26cde1e8381d847aed84be29d3&\\_phsrc=vMS848&\\_phstart=successSource](https://www.ancestry.com/discoveryui-content/view/30255485:2272?tid=&pid=&queryId=dae45b26cde1e8381d847aed84be29d3&_phsrc=vMS848&_phstart=successSource); “Certificate of Death for Charles Early Martin” (Texas State Board of Health, June 19, 1956), Ancestry.com, [https://www.ancestry.com/discoveryui-content/view/21615447:2272?tid=&pid=&queryId=dae45b26cde1e8381d847aed84be29d3&\\_phsrc=vMS848&\\_phstart=successSource](https://www.ancestry.com/discoveryui-content/view/21615447:2272?tid=&pid=&queryId=dae45b26cde1e8381d847aed84be29d3&_phsrc=vMS848&_phstart=successSource); “World War I Draft Registration Card for Charles Early Martin,” June 5, 1917, Ancestry.com, [https://www.ancestry.com/discoveryui-content/view/17813959:6482?tid=&pid=&queryId=dae45b26cde1e8381d847aed84be29d3&\\_phsrc=vMS848&\\_phstart=successSource](https://www.ancestry.com/discoveryui-content/view/17813959:6482?tid=&pid=&queryId=dae45b26cde1e8381d847aed84be29d3&_phsrc=vMS848&_phstart=successSource).

<sup>38</sup> *Polk’s Morrison & Fourmy Austin City Directory* (Austin, Texas: Morrison & Fourmy Directory Co., Inc., 1924), 103, 325, [https://www.ancestry.com/imageviewer/collections/2469/images/5017747?usePUB=true&\\_phsrc=vMS851&\\_phstart=successSource&usePUBJs=true&pId=404187596](https://www.ancestry.com/imageviewer/collections/2469/images/5017747?usePUB=true&_phsrc=vMS851&_phstart=successSource&usePUBJs=true&pId=404187596); “Leo Martin Last Rites Set Today,” *The Austin American*, January 21, 1957, Newspapers.com.

<b>POULTRY REMEDIES AND SUPPLIES</b>	
<p>Martin's Sanitary Brands of Floor Sweep, Insecticide, Floor Oil, Liquid Soap, Soluble Disinfectants, Disinfectant Powders, Pine Disinfectants, Drip Fluid, Sure Death Bug Killer, Perfumed Theatre Spray, Toilet Paper, Paper Towels, Fixtures for Liquid Soap, Toilet Paper and Paper Towels.</p>	<h1 style="margin: 0;">C. J. Martin &amp; Sons</h1> <h2 style="margin: 0;">SEEDSMEN</h2> <p style="margin: 0;">Manufacturers and Jobbers</p> <h3 style="margin: 0;">Sanitary Chemicals and Supplies, Stock Dip and Poultry Remedies and Supplies</h3> <p style="margin: 0;">OFFICE and RETAIL STORE 112-18 E. 4th.      TEL. 6064  FACTORY, 815 E. FOURTH ST.                      TEL. 8821</p>

Figure 9. An advertisement for C.J. Martin & Sons in the 1924 Austin city directory. Note the factory address of 815 East 4<sup>th</sup> Street, a former street number matching the current location of the subject property at 807 East 4<sup>th</sup> Street.<sup>39</sup>

According to TCAD assessor records, an addition was built on the west and south elevations of the building in 1937. However, the one-story frame addition appears on the 1935 Sanborn map (see Figure 10), which shows the original two-story stone building as a chemical compound storage building. It is likely the addition was built to meet the storage needs of the growing and flourishing C.J. Martin & Sons business. The addition appears in historic aerials from 1952 (see Figure 11).

Charles J. Martin died in 1926 and his sons continued to run the business until the late 1940s, when they sold the company to Werner J. Perlitz.<sup>40</sup> By 1952 C.J. Martin & Sons had vacated the building and Cabaniss Furniture Company had moved in.<sup>41</sup> The Cabaniss Furniture Company was founded in 1920 by Charles M. Cabaniss and John Wall. Within a few months Wall sold his interest in the company to Cabaniss's nephew, Bert L. Cabaniss, and J.S. (Steen) Brown.<sup>42</sup> According to a newspaper article published in *The Austin American* on October 18, 1953, Cabaniss was "one of the largest furniture firms in this section of the Southwest."<sup>43</sup> This same article describes the company as having its main show room located at 200-204 East 6<sup>th</sup> Street (extant), a warehouse salesroom at San Jacinto Boulevard and East 5<sup>th</sup> Street, a used furniture store known as Brown Furniture Company at 218 East 6<sup>th</sup> Street (extant), and their warehouse

<sup>39</sup> Polk's Morrison & Fourmy Austin City Directory, 1924, 103.

<sup>40</sup> "Werner J. Perlitz," *Austin American-Statesman*, December 12, 1993, Newspapers.com.

<sup>41</sup> HHM, Inc., *City of Austin Historic Resources Survey: Final Report, Volume IV*, 44.

<sup>42</sup> "Cabaniss Tells of Ways to Win Home Tour Contests," *The Austin Statesman*, May 2, 1931, Newspapers.com; "Cabaniss-Brown Shows Growth in 3rd Century," *The American-Statesman*, October 18, 1953, Newspapers.com.

<sup>43</sup> "Cabaniss-Brown Shows Growth in 3rd Century."

located at East 4<sup>th</sup> Street and Brushy Street (the subject property). At the time the article was published, the company was constructing a new addition to the warehouse, doubling its floor space (see Figures 12 and 13). The new addition, located on the west side of the c.1935 addition, would allow the San Jacinto Boulevard and East 5<sup>th</sup> Street location to operate as a salesroom only.<sup>44</sup>

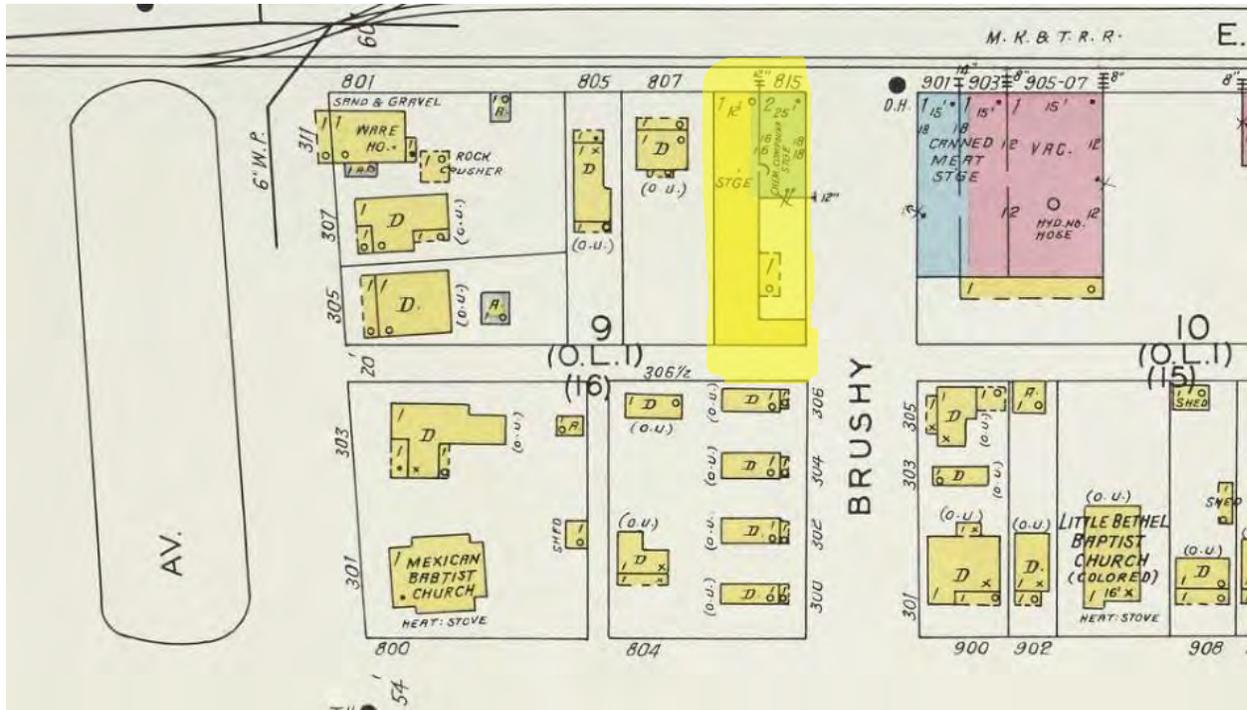


Figure 10. 1935 Sanborn map showing the Walker Brothers Warehouse (highlighted in yellow) with the one-story, frame, c.1935 addition on the side (west) elevation. Note the median from East Avenue (now I-35).<sup>45</sup>

<sup>44</sup> “Cabaniss-Brown Shows Growth in 3rd Century.”

<sup>45</sup> “Austin, Texas, 1935 Vol. 1.,” 1:600 (New York: Sanborn Map Company, 1935), Sheet 211, Library of Congress, [https://www.loc.gov/item/sanborn08415\\_006/](https://www.loc.gov/item/sanborn08415_006/).



Figure 11. The Walker Brothers Warehouse in September 1952. Note the c.1935, one-story addition around the original two-story building and East Avenue to the west.<sup>46</sup>

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<sup>46</sup> United States Geological Survey, "Aerial Image 1XF0000010104" (Department of Interior, US Geological Survey, September 28, 1952), [earthexplorer.usgs.gov](https://earthexplorer.usgs.gov), <https://earthexplorer.usgs.gov/>.



Figure 12. This December 1959 photo shows the Cabaniss Furniture Company in the building (red arrow). Note the demolition of the single-family homes to the southwest of the warehouse in preparation of I-35 construction, as well as the East Avenue median in the foreground.<sup>47</sup>



Figure 13. Zoomed-in version of the December 1959 photograph in Figure 12. Note the original two-story, c.1880 building highlighted in blue, the c.1935 addition highlighted in grey, and the c.1953 one-story addition highlighted in purple.<sup>48</sup>

<sup>47</sup> Texas Department of Transportation, *IH 35 Construction at East 4th Street*, December 1959, TxDOT Photo Library.

<sup>48</sup> Texas Department of Transportation, *IH 35 Construction at East 4th Street*.

In 1962 the warehouse was vacant, according to the HHM, Inc. 2016 survey. By the late 1960s the Pool Transfer & Storage Company, Inc. occupied it (see Figures 14-18).<sup>49</sup> Pool Transfer & Storage Company was founded in Austin in the 1910s by Alfred Houston Pool. After his death in 1932, his son Hill Houston (H.H.) became instrumental in continuing the business. In addition to acting as agents of the Global Van Lines, Pool Transfer & Storage Company held auctions at the subject property, often auctioning seized property it moved to the warehouse when the owners of the property did not pay the necessary fines to keep their possessions. The company also held auctions of goods like television sets and Persian rugs at the subject property.<sup>50</sup> By 1973 H.H. acquired the lots to the south of the subject property with the single-family homes that would later become a courtyard, ad-hoc storage, and a parking lot.<sup>51</sup> Although it is unknown when Bert L. Cabaniss purchased the lots to the north that include the subject property, Travis County Clerk and Recorder records indicate Cabaniss sold the property to H.H. Pool, H.H. Pool, Jr., and H.H. Pool's son-in-law James J. Stahl in 1983.<sup>52</sup> I-35 was constructed through this area along former East Avenue in the early 1960s. By the time of the 1962 Sanborn map update (see Figures 5 and 6), the highway was in place and several buildings and structures had been removed to make way for the frontage road, including the houses formerly located on the western portion of the 800 block of East 4<sup>th</sup> Street and buildings southwest of the Walker Brothers Warehouse, including a "Mexican Baptist Church."<sup>53</sup>

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<sup>49</sup> HHM, Inc., *City of Austin Historic Resources Survey: Final Report, Volume IV*, 44.

<sup>50</sup> "LOST," *Austin Daily Statesman*, February 17, 1915, sec. Classified, Newspapers.com; *Polk's Morrison & Fourmy Austin City Directory* (Austin, Texas: Morrison & Fourmy Directory Co., Inc., 1922), 537, <https://texashistory.unt.edu/ark:/67531/metaph39153/m1/5/>; "World War I Draft Registration Card for Alfred Houston Pool," September 12, 1918, Ancestry.com, <https://www.ancestry.com/discoveryui-content/view/17814878:6482>; "Alfred Houston Pool," *Austin American*, June 1, 1932, Newspapers.com, <https://www.newspapers.com/image/385909442/>; "Auction Notice," *Austin Statesman*, July 12, 1968, Newspapers.com; "Auction," *Austin American-Statesman*, August 16, 1979, Newspapers.com; "H.H. 'Buster' Pool," *Austin American-Statesman*, July 31, 1994, sec. Deaths and funerals, Newspapers.com.

<sup>51</sup> "Deed, H.H. Pool et al and Hazel Pool et Al," April 11, 1997, Vol. 12925, Page 00167, Register of Deeds Office, Travis County, Texas; "Deed, Hazel Pool et al and Katie Rose Investments LTD," August 3, 1998, Vol. 13238, Page 02436, Register of Deeds Office, Travis County, Texas.

<sup>52</sup> "Release of Lien between Bert L. Cabaniss and H.H. Pool, H.H. Pool, Jr., and James J. Stahl," June 6, 1983, Vol. 8114, Page 92, County Clerk Web Search, Travis County, Texas; "H.H. 'Buster' Pool."

<sup>53</sup> "Austin, Texas, 1935 Vol. 1.," Sheet 211.



Figure 14. This 1971 photo of the Walker Brothers Warehouse shows the original two-story stone building and the part of the c.1935 addition on the west side of the original c.1880 building. Note the changes to the fenestration on the front (north) facade.<sup>54</sup>



Figures 15 and 16. 1980 photographs of the Walker Brothers Warehouse front (north) facade and details of the entry on the front facade.<sup>55</sup>

<sup>54</sup> Carl [Horzberger], *HB - 4th E. 807*, Photograph, black & white, 1971, Austin History Center.

<sup>55</sup> *Historic Property, Photograph THC\_06-0245*, Photograph, black & white, April 27, 1980, The Portal to Texas History, <https://texashistory.unt.edu/ark:/67531/metaph677100/>; *Historic Property, Photograph THC\_06-0244*, Photograph, black & white, April 27, 1980, The Portal to Texas History, <https://texashistory.unt.edu/ark:/67531/metaph676746/>.



Figures 17 and 18. 1980 photographs of the Walker Brothers Warehouse front (north) facade and side (east) elevation.<sup>56</sup>

The owners of Pool Transfer & Storage owned the building and the parcel to the south until they combined the parcels and sold the property to Katie Rose Investments in 1998. In 2004 Cothron Safe & Lock Company purchased the property. Cothron Safe & Lock was located there until Anchor Equities, Ltd. purchased the property in 2014.<sup>57</sup> Under Anchor Equities' ownership, the building hosted a variety of businesses, including a hostel in the original two-story stone building; and an event space, coffee shop, bar, café, music venue, and tattoo parlor in the two additions.<sup>58</sup> The exterior of the additions also have periodically changing murals painted by various artists, including local Austinites Sophie Roach in 2018, Mylo Mendez in 2020, Chris Rogers in 2021, and Sarah Wilson in 2021.<sup>59</sup> Currently the additions are host to murals by artists Mylo Mendez, Chris Rogers, Sarah Wilson, and Brooklyn-based artist Jason Naylor.<sup>60</sup>

<sup>56</sup> *Historic Property, Photograph THC\_06-0242*, Photograph, black & white, April 27, 1980, The Portal to Texas History, <https://texashistory.unt.edu/ark:/67531/metaph676023/>; *Historic Property, Photograph THC\_06-0243*, Photograph, black & white, April 27, 1980, The Portal to Texas History, <https://texashistory.unt.edu/ark:/67531/metaph673245/>.

<sup>57</sup> "Deed, H.H. Pool et al and Hazel Pool et al"; "Deed, Hazel Pool et al and Katie Rose Investments LTD"; "Deed, Katie Rose Investments LTD and Cothron Safe & Lock Company Inc.," August 3, 1998, Instrument No. 2004096418TR, Register of Deeds Office, Travis County, Texas; "Deed, Cothron Safe & Lock Company Inc. and Cothron Land & Cattle Co LP," December 17, 2013, Instrument No. 2013226988TR, Register of Deeds Office, Travis County, Texas; "Deed, Cothron Land & Cattle Co LP and Anchor Equities LTD," January 10, 2014, Instrument No. 2014005392TR, Register of Deeds Office, Travis County, Texas.

<sup>58</sup> "A New Way to Experience Hospitality," *Native*, n.d., <https://nativehostels.com/>.

<sup>59</sup> @nativehostel, "The Process," Social Media, *Instagram*, (July 17, 2018), <https://www.instagram.com/p/BIVdowVjcbN/>; "About," *Sophie Roach*, n.d., <https://sophieroach.com/about/>; "Native Hostel," *Sophie Roach*, 2018, <https://sophieroach.com/native-hostel-mural/>; Hank Cavagnaro, "Austin Muralist Honoring Essential Women Workers with Art around the City," *KVUE*, February 1, 2022, <https://www.kvue.com/article/news/community/austin-muralist-essential-women-workers-art/269-e9f5d01c-254f-48c9-ad59-3a74244cea83?msclkid=113884d4aeb211ecb6989340b226e716>; "About Sarah," *Sarah Wilson Photography*, n.d., <http://www.sarahwilsonphotography.com/about-sarah/>; @handmademendez, "Repost from @fbairprogram," Social Media, *Instagram*, (November 13, 2020), <https://www.instagram.com/p/CHJdQqEIW6C/>; "About," *Chris Rogers Art*, 2020, <https://www.chrisrogersartist.com/about/>; @chrisrogersart, "#Repost @npr @KUTX Examines the Impact of Austin's Racial Justice Protests Inspired by George Floyd's Death," Social Media, *Instagram*, (February 15, 2021), <https://www.instagram.com/p/CLVaQJYFoOc/>.

<sup>60</sup> "Creatively Positive X Positively Creative," *Jason Naylor*, 2020, <https://jasonnaylor.nyc/about>.

The idea to convert the warehouse into a hostel came from Michael Dickson, Antonio Madrid, and Will Steakley, co-owners and investors of Native Hostel.<sup>61</sup> The investment group and their interior designers decided to rehabilitate the warehouse, keeping original elements of the building, including the rough-stone exterior and exposed ceilings and plaster interior walls (see Figure 19).<sup>62</sup> The project transformed the original two-story stone building into a hostel with 12 bedrooms, 68 beds, a coffee shop, bar, and restaurant, and equipped the additions to house other businesses and a large event space. Native Hostel opened its doors in May 2017.<sup>63</sup> Today Google shows the hostel as permanently closed.<sup>64</sup>



Figure 19. The upstairs interior of the Native Hostel “Romper Room” in March 2021.<sup>65</sup>

The current legal parcel includes lots 7-10 to the south of the Walker Brothers Warehouse. Historically, however, these lots were not associated with the warehouse. Sanborn maps from as early as 1894 show long narrow lots running north to south on block 16 of division O,

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<sup>61</sup> Dan Zehr, “Rules Let Small Givers Fund Some Big Dreams,” *Austin American-Statesman*, June 12, 2016, sec. Business Sunday, Newspapers.com.

<sup>62</sup> Zehr, “Rules Let Small Givers Fund Some Big Dreams”; Jay Janner, “The Bluest Eyes in Texas,” *Austin American-Statesman*, May 19, 2017, sec. Metro & State, Newspapers.com; Tobin Levy, “Dark and Stormy,” *Tribeza*, October 2017, <https://tribeza.com/dark-and-stormy/>.

<sup>63</sup> Janner, “The Bluest Eyes in Texas”; Levy, “Dark and Stormy.”

<sup>64</sup> “Street View of 807 East 4th Street, Austin, TX (Jan 2022),” *Google Maps*, 2022, <https://goo.gl/maps/CWvhGxZ4Ku9nGMxK6>; “Property 191640,” *Travis Central Appraisal District*, 2022, <https://stage.travis.prodigycaad.com/property-detail/191640/2022>; “Jason Nassour,” *Keel & Nassour, L.L.P.*, 2020, <https://defendtexans.com/about-us/jason-nassour/>.

<sup>65</sup> “Events,” *Native*, n.d., <https://nativehostels.com/events>.

outlot 1, with an alley running west to east dividing lots 1-10 from lots 11-20 (see Figure 20). By 1935 the orientation of the houses ran west to east and lots 7-10 were redrawn to become one large lot with multiple single-family homes on it (see Figure 21). The large lot had multiple single-family homes on it from the 1930s until the early 1980s, according to Sanborn maps and historic aerials.<sup>66</sup> City directories from as early as 1916 list families with Latino surnames living in the homes on this lot until the late 1960s. Research in the Travis County Clerk and Recorder information indicates none of these families owning the property, implying the houses were likely rentals. In 1973 H.H. Pool acquired the lots. By 1981 the homes had been demolished and storage containers filled the space, and by 1985 lots 7-10 were vacant.<sup>67</sup> Today Lots 7-10 act as a courtyard for the hostel and commercial businesses, ad-hoc storage, and parking, and are surrounded by non-historic-age metal fencing

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<sup>66</sup> "Austin, Texas, June 1885," Sheet 7; "Austin, Travis County, Texas, January 1894," Sheet 20; "Austin, Texas, 1900," 1:600 (New York: Sanborn Map Company, 1900), Sheet 12, Digital Sanborn Maps, 1867-1970, ProQuest; "Austin, Texas, 1935 Vol. 1.," Sheet 211; "Austin, Texas, 1935 (Revised 1962) Vol. 2.," Sheet 207; United States Geological Survey, "Aerial Image 1XF0000010104"; "Aerial Image, Austin, Texas, 1973," 1973, Historic Aerials by NETR Online, <https://www.historicaerials.com/viewer>; "Aerial Image, Austin, Texas, 1981," 1981, Historic Aerials by NETR Online, <https://www.historicaerials.com/viewer>; "Aerial Image, Austin, Texas, 1985," 1985, Historic Aerials by NETR Online, <https://www.historicaerials.com/viewer>.

<sup>67</sup> "Aerial Image, Austin, Texas, 1973"; "Aerial Image, Austin, Texas, 1981"; "Aerial Image, Austin, Texas, 1985."

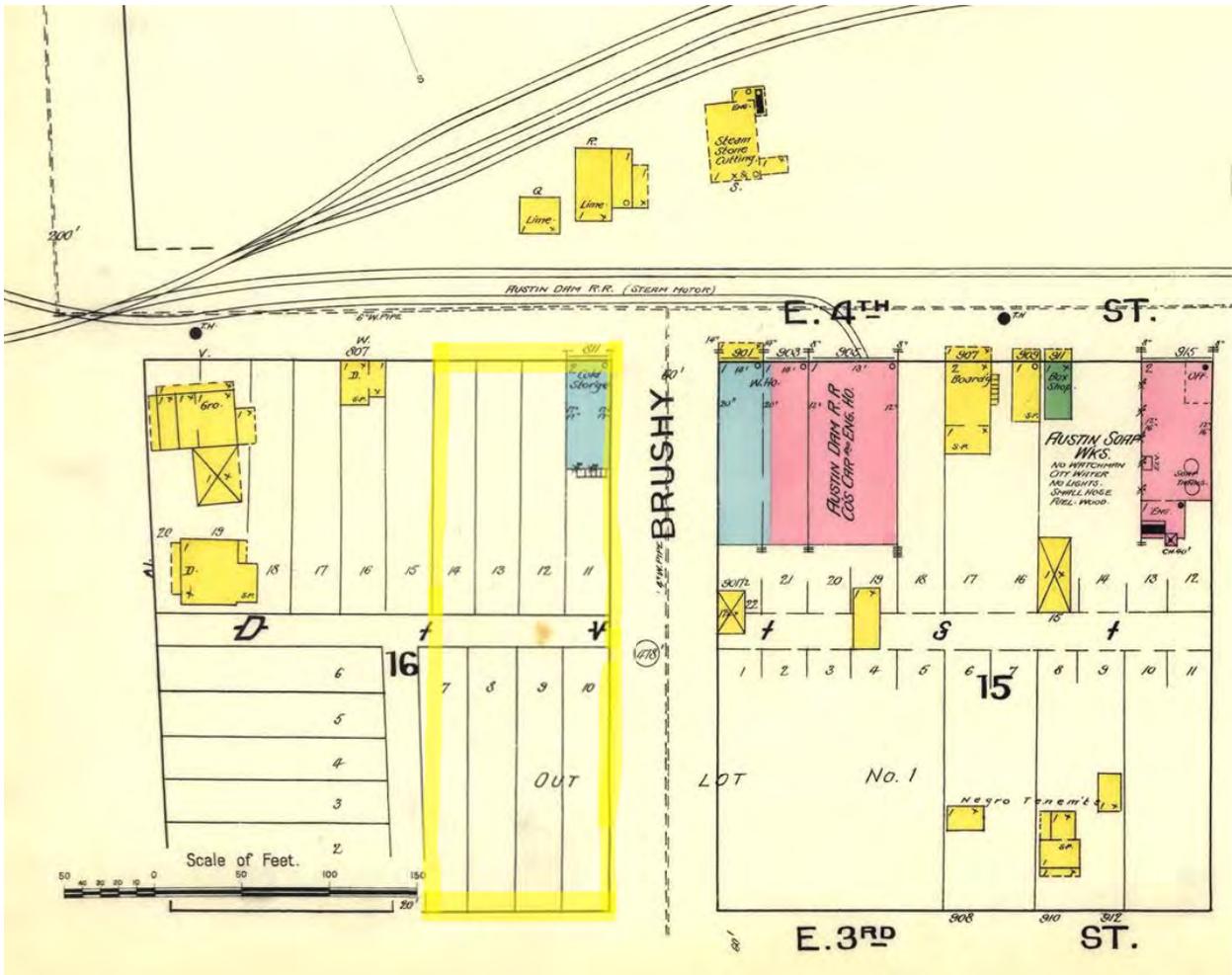


Figure 20. 1894 Sanborn Map showing the Walker Brothers Warehouse on lot 11 of block 16, division O, outlot 1. The bright yellow highlighted rectangle shows the current legal boundary of the property.<sup>68</sup>

<sup>68</sup> "Austin, Travis County, Texas, January 1894," Sheet 20.

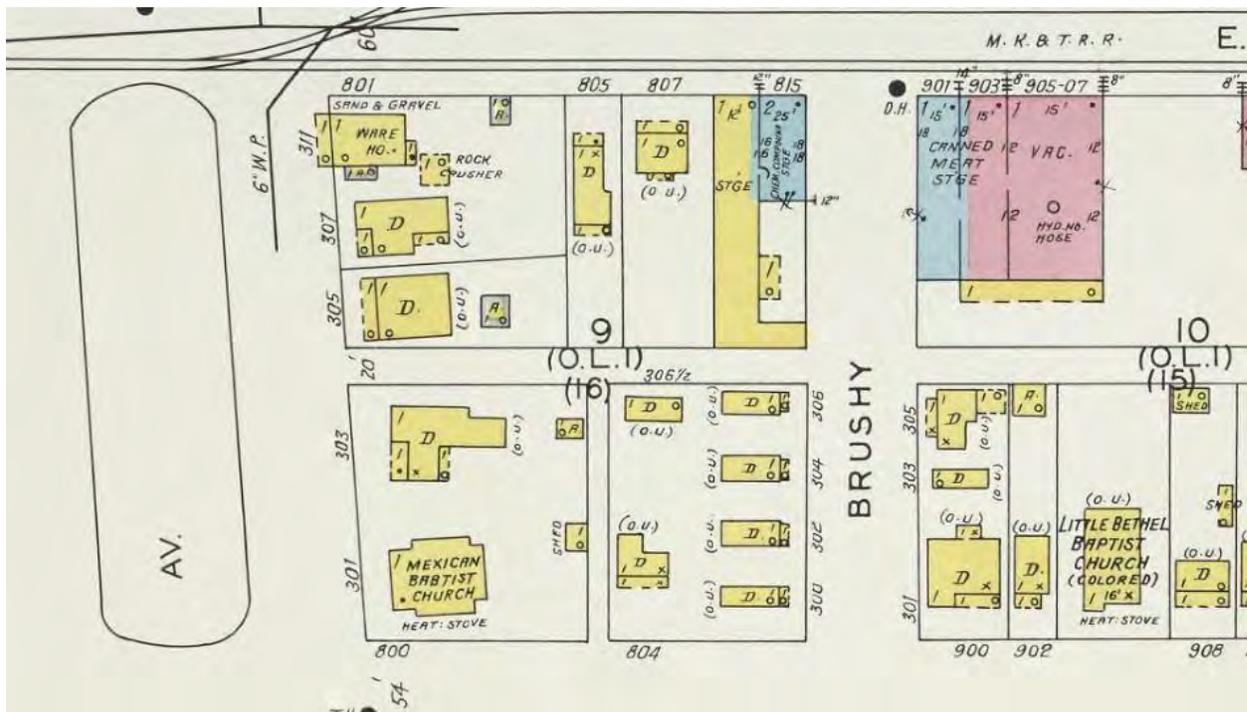


Figure 21. 1935 Sanborn map showing the single-family homes to the south of the Walker Brothers Warehouse. Note the large lot now encompassing lots 7-10 shown in the 1894 Sanborn map and the change in orientation of the homes now facing Brushy Street and East 3<sup>rd</sup> Street.<sup>69</sup>

## National Register Eligibility Recommendations

- Eligible Properties/Districts

### Resource 400: Walker Brothers Warehouse, 807 East 4<sup>th</sup> Street

The Walker Brothers Warehouse was built c.1880, with c.1935 and c.1953 additions. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-light, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. A flat corrugated metal awning suspended from metal anchors wraps around the front and side (east) elevation. The building occupies what remains of the city block that is bounded by East 3<sup>rd</sup> Street to the south, Brushy Street to the east, the I-35 northbound frontage road to the west, and East 4<sup>th</sup> Street to the north. A sidewalk borders the west, north, and east sides of the building with a raised portion with ramps and a metal and wood railing along the east side. Metal planters are located on the sidewalk along the front (north) facade, and grass flanks the sidewalk along the side (west) elevation of the building. A courtyard with raised wood planters and deciduous trees is located to the south of the building.

<sup>69</sup> "Austin, Texas, 1935 Vol. 1.," Sheet 211.

The c.1880 portion of the front (north) facade is three bays across with an entrance flanked by two fixed metal windows. The entrance consists of a glazed single metal door with fixed metal sidelights. The three second-story window openings feature flat limestone sills and rusticated limestone segmental arch lintels. The roofline has minimal limestone detailing along the cornice.

The east (side) elevation is three bays across with an entrance on the southernmost bay. The entrance consists of a glazed single metal door with fixed metal sidelights that match the facade. Windows consist of four-over-four-light wood sash. Flat limestone sills and segmental arch lintels highlight door and window openings. The second-story rear (south) elevation is three bays across with three windows infilled with brick. Segmental arch lintels are above the altered fenestration.

A one-story, flat-roof, brick c. 1935 addition with a poured concrete foundation replaced the first story of the side (west) elevation and wraps around the c.1880 building mass to the rear (south) elevation. A recessed entrance on the front (north) facade of the addition consists of a glazed single metal door with fixed metal windows and exposed metal rafters and a metal lintel above the opening. A billboard facing I-35 sits above the addition and on the side (east) elevation of the original two-story stone building. The side (east) elevation of the addition has three windows and no entrances. The windows are glass block with brick sills and metal bars covering the openings. The roofline is a stepped parapet with metal coping. The rear (south) elevation consists of fixed metal windows and glass single door and glass double door openings out to a courtyard.

A one-story, flat-roof, brick, c.1953 addition with a poured concrete foundation is attached to the side (west) elevation of the c.1935 addition (see Figure 22). An entrance with a single-leaf metal door is on the front (north) facade of the addition. The side (west) elevation has two recessed entrances with single-leaf metal doors. The roofline is a stepped parapet with metal coping. A metal sign sits on the southeast corner of the roofline facing I-35.

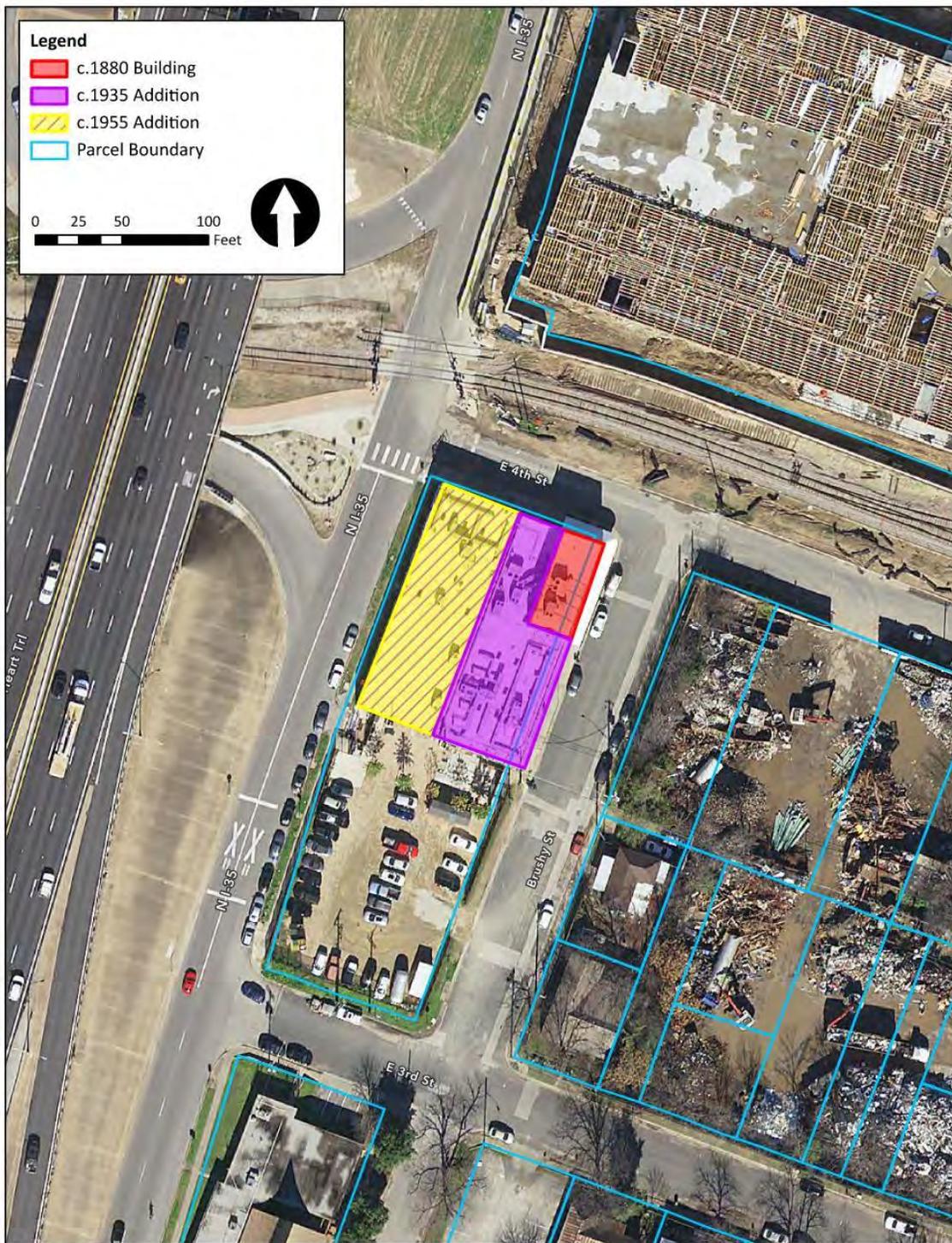


Figure 22. Texas Natural Resources Information System StratMap image with the original building and historic-age additions highlighted to show the progression of build dates at 807 East 4<sup>th</sup> Street.<sup>70</sup>

<sup>70</sup> "Aerial Image, Travis County, 2021" (Google Earth, 2021), Google Earth Pro.

The warehouse has undergone several alterations since its c.1880 construction. Historic-age additions were added to the side (west) elevation c.1935 and c.1953. Sometime between its construction and 1959 the main entrance on the original building's front (north) facade was replaced with a metal overhead door and the first-story windows were infilled, as seen in Figures 12-13. Between 1971 and 1980 the windows on the c.1880 portion of the building were replaced with metal sash windows. Google Street View images from 2007 show the windows restored to four-over-four-light wood sash and the infilled windows reinstated with fixed metal windows on the front (north) facade.<sup>71</sup> Between February and May 2017 the current owner added glazed metal entry doors.<sup>72</sup> On the c.1935 addition the current owner reopened the previously infilled entrance on the front (north) facade, infilled an entrance and single bay overhead door on the side (east) elevation, and added the fixed metal windows and entrances on the rear (south) elevation. They also infilled a single bay overhead door on the front (north) facade of the c.1955 addition. Between 2015 and 2017 the owners painted the brick on the additions black and added the metal awning on the front (north) facade and side (east) elevation of the original building. The murals on the additions appear by 2019 and have been completely replaced with new artwork between then and January 2022.<sup>73</sup> The interior of the former warehouse has been converted into a hostel, commercial spaces, and event space/music venue while preserving select elements of the warehouse such as the exposed ceilings and plaster walls.<sup>74</sup>

## **Significance**

### *Criterion A*

Based on historic contexts, the Walker Brothers Warehouse is significant under NRHP Criterion A in the area of Industry at the local level of significance for its important association with industrial and commercial storage and its role as one of the only remaining industrial properties along the former H&TC Railroad corridor. The warehouse was one of a collection of storage and processing facilities that once lined the railroad in East Austin and represented the city's thriving industrial growth in the early twentieth century. The Walker Brothers Warehouse is the only such property in its immediate vicinity, and is among only a handful of extant warehouses or industrial buildings along former railroad corridors through downtown and East Austin. The only extant industrial building near the subject property is the main warehouse and office building of the Austin Metal & Iron Company (Resource 403A-C, 300 Medina Street) built in 1913 with additional buildings added to the complex c.1920 and in 1973. Of the two industrial buildings on East 4<sup>th</sup> Street, the subject property best conveys its initial industrial purpose and

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<sup>71</sup> "Street View of 807 East 4th Street, Austin, TX (2007, 2009, 2011, 2013, 2014, 2015, 2017, 2019, 2021, 2022)," *Google Maps*, accessed March 17, 2022, <https://goo.gl/maps/mrXZk849vDa8Bx6Y9>.

<sup>72</sup> HHM, Inc., *City of Austin Historic Resources Survey: Final Report, Volume IV*, 43.

<sup>73</sup> "Street View of 807 East 4th Street, Austin, TX (2007, 2009, 2011, 2013, 2014, 2015, 2017, 2019, 2021, 2022)."

<sup>74</sup> Zehr, "Rules Let Small Givers Fund Some Big Dreams"; Janner, "The Bluest Eyes in Texas"; Levy, "Dark and Stormy"; "A New Way to Experience Hospitality."

appearance, having retained its original stone construction and form. The period of significance runs from c.1880, when the warehouse was constructed, until 1998, when the owners of Pool Transfer & Storage sold it to Katie Rose Investments and the building has had a series of shorter-term tenants that no longer use the building as a warehouse.

### *Criterion B*

According to the NRHP bulletin *How to Apply the National Register Criteria for Evaluation*, a property can be significant under Criterion B if it illustrates the “important achievements” of a person “whose specific contributions to history can be identified and documented.”<sup>75</sup> Although W.B. and James J. Walker were early businessmen and grocers in Austin, research did not reveal significant achievements that would set them apart from their peers in Austin’s business and social circles. While they owned a successful grocery warehouse, this was one of many thriving businesses along the railroad corridor and their company was one of many grocers in Austin. Therefore, the subject property does not possess significance under Criterion B for its association with the Walkers. Research did not indicate historic associations with other significant figures in local, state, or national history. Therefore, the Walker Brothers Warehouse does not possess NRHP significance under Criterion B.

### *Criterion C*

The Walker Brothers Warehouse was evaluated for potential significance under NRHP Criterion C for design/construction. According to the NRHP bulletin *How to Apply the National Register Criteria for Evaluation*, a property can be significant under Criterion C if it “embodies distinctive characteristics of a type, period, or method of construction; represents the work of a master; or possesses high artistic value.”<sup>76</sup> As a standard and utilitarian warehouse building, the subject property does not possess high artistic value or represent the work of a master. It does, however, display characteristic warehouse features, including its two-story layout and sprawling, one-story, historic-age additions, as well as its proximity to the former railroad corridor. As such, the property is significant under Criterion C at the local level of significance, as a rare extant example of a one-part, two-story commercial and industrial warehouse, a once-common property type in this area.

### *Integrity*

The original c.1880 Walker Brothers Warehouse has two additions; while not original, they are of historic age and are within the period of significance. The original building and historic-age additions stand in their original location, retaining integrity of location. The original c.1880 building has strong integrity of design, retaining its original form and modest design detailing such as the decorative limestone sills and segmental arch lintels around the window openings. The historic-age additions also have integrity of design, as they retain their original form and

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<sup>75</sup> National Park Service, *How to Apply the National Register Criteria for Evaluation* (Washington, D.C.: Department of the Interior, 1997), 14.

<sup>76</sup> National Park Service, *How to Apply the National Register Criteria for Evaluation*, 17.

brick details. Although the brick has been painted black and is covered in murals, the historic-age additions have integrity of materials and workmanship as the details of the brick are still visible. The original c.1880 building also has integrity of materials and workmanship, displaying historic limestone materials and building methods with tool marks still visible on the exterior walls of the building. Some other building materials, including doors and windows on the original c.1880 building, have been replaced; warehouse garage bays have been infilled in the historic-age additions; and the interior of the original building and historic-age additions have been renovated into a hostel, businesses, and event space/music venue. Therefore, the building and its additions have some diminished integrity of materials and workmanship. Modern development, including the construction of I-35 and new construction to the north and northeast of the warehouse, have compromised the historic setting of the area. However, the Austin Metal & Iron Co. warehouses and facilities to the east of the subject property along East 4<sup>th</sup> Street and the historic-age residential properties to the southeast do contribute to integrity of feeling and association. Overall, the property retains sufficient integrity to convey its historic significance under Criterion A as a former industrial warehouse.

Although the property has undergone architectural changes, including the removal and replacement of doors and windows and renovations to the original interior warehouse space to convert it into a hostel, commercial spaces, and an event space/music venue, the replacement of materials was sensitive to the historic fenestration and does not diminish the overall integrity of the property. Therefore, the property retains sufficient integrity to convey its historic significance under Criterion C as a rare example of a one-part two-story warehouse in Austin.

### ***NRHP Boundaries***

The Walker Brothers Warehouse is recommended eligible under Criterion A in the area of Industry for its association with industrial development along the H&TC Railroad and under Criterion C in the area of Architecture. According to the NRHP bulletin *How to Complete the National Register Registration Form*, boundaries for historic sites should be selected to “encompass the area where the historic events took place.”<sup>77</sup> This corresponds to the building footprint, including the c.1880 original building and c.1935 and c.1953 additions. It does not include the courtyard and parking lot to the south, as those lots were historically comprised of single-family homes on separate lots until the 1980s and not associated with the warehouse building.

### ▪ **Ineligible Properties/Districts**

None.

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<sup>77</sup> National Park Service, *National Register Bulletin: How to Complete the National Register Registration Form*, 1997, 56, <https://www.nps.gov/subjects/nationalregister/upload/NRB16A-Complete.pdf>.

- **Recommendations for Further Study**

Evaluations of NRHP eligibility can be made from existing project information and the findings of the intensive-level survey, as documented in this report. No further work is recommended at this time.

## **Determination of Section 106 Effects Recommendations**

- **Direct Effects**

### **Resource 400: Walker Brothers Warehouse**

The Walker Brothers Warehouse is recommended eligible for the NRHP under Criteria A and C. The NRHP boundaries do not conform to the TCAD parcel boundaries, as they exclude the courtyard and parking lot to the south of the building. At this location, I-35 currently has three main lanes in each direction of traffic crossing over the Capital Metro Red Line railroad tracks, as well as a two-lane, at-grade, northbound frontage road. The railroad is currently at-grade at this location. The main lanes are elevated on fill approximately 15 feet above grade. The Walker Brothers Warehouse building extends to the west edge of the associated property parcel and directly abuts the I-35 ROW at the northbound frontage road. The Walker Brothers property boundary and building face are approximately 10 feet from the northbound frontage road pavement edge and approximately 125 feet from the closest I-35 main lane.

### ***Alternative 2***

At this location, I-35 would be reconstructed to include four main lanes and two managed lanes in each direction. The main lanes and managed lanes would be depressed below grade. At East 4<sup>th</sup> Street, there would be a one-lane, depressed, northbound frontage road on the east side of the I-35 ROW, with a ramp carrying traffic from the northbound bypass lane to the northbound frontage road. Just north of East 4<sup>th</sup> Street, the northbound frontage road widens to three lanes. A one-lane southbound frontage road and a ramp between the southbound frontage road and southbound bypass lane, also depressed, are located on the west side of the I-35 ROW. Shared-use paths would be located on the east and west edge of the I-35 ROW (see Figures 23 and 24).

The Capital Metro Red Line tracks would be carried on a bridge structure over the depressed vehicular lanes. The Capital Metro Red Line will be approximately six feet above existing ground level at the edge of the I-35 ROW and will lower from west to east, matching the existing rail profile just west of San Marcos Street. A separate pedestrian bridge, adjacent to the south side of the railroad bridge, would carry a shared-use path over the depressed vehicular lanes. Vehicular access from East 4<sup>th</sup> Street to northbound I-35 would be lost. However, as noted above, pedestrian/bicycle access across I-35 at East 4<sup>th</sup> Street would be maintained with construction of a pedestrian bridge and shared-use path. The nearest vehicular access to the I-35 northbound frontage road would be provided at East 3<sup>rd</sup> Street. There is potential for construction of a concrete cap over the depressed main lanes and managed lanes from the Capital Metro Red Line bridge north to East 8<sup>th</sup> Street.

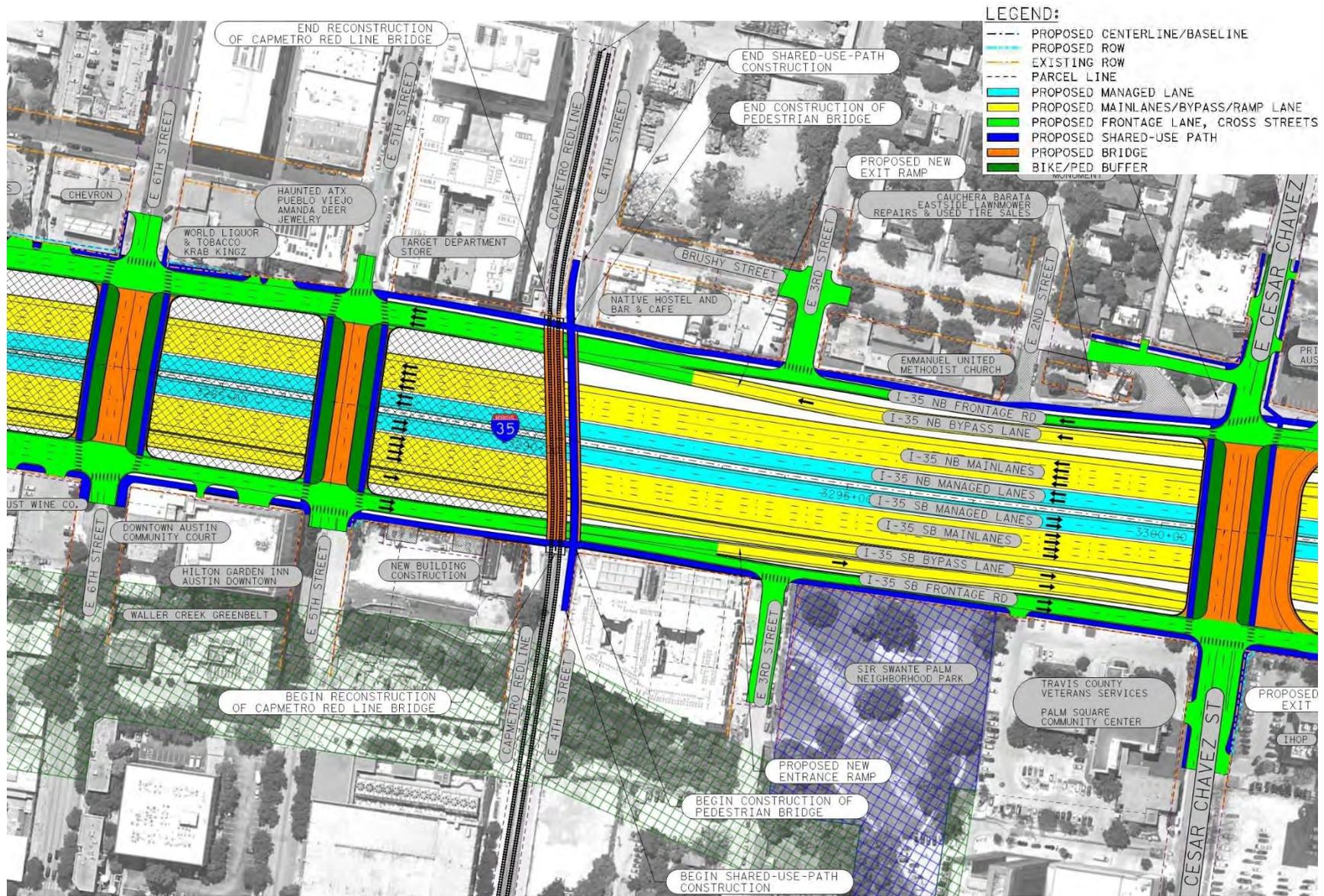
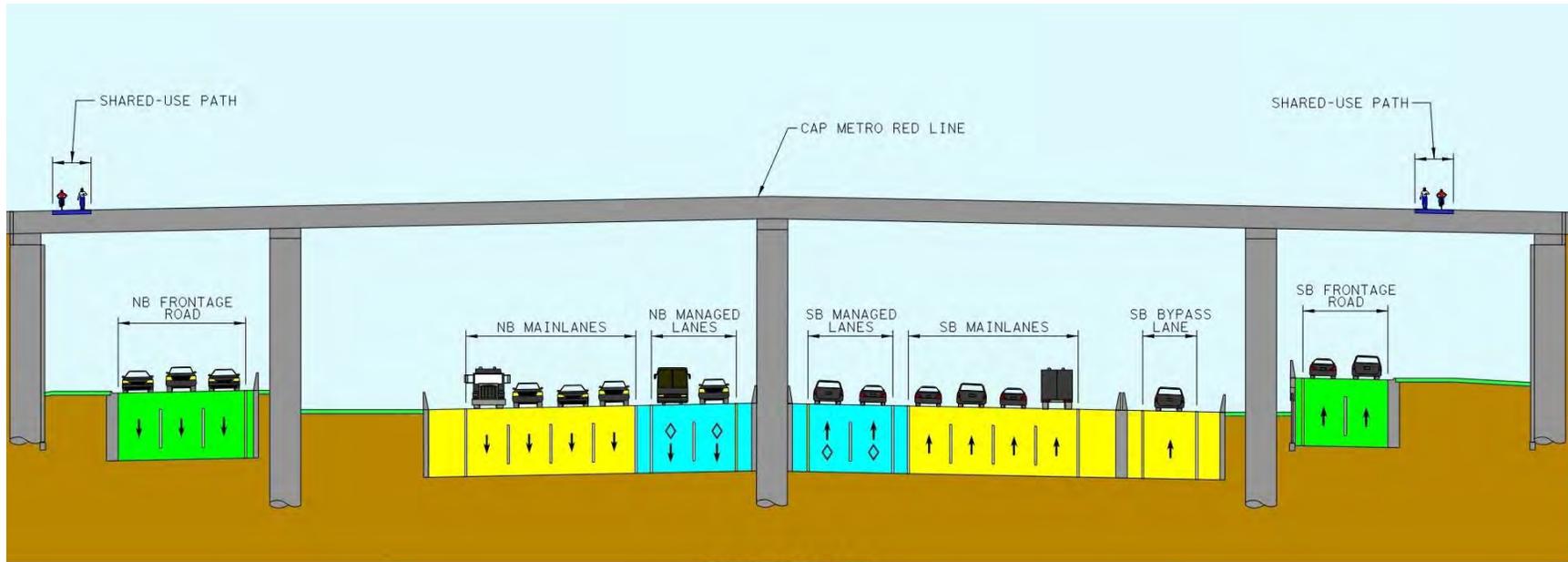


Figure 23. Close-up of Alternative 2 project schematic (April 8, 2022, version) showing proposed configuration in the Walker Brothers Warehouse vicinity. Note: the Walker Brothers Warehouse is labeled as Native Hostel on this image.



**SECTION I-I**

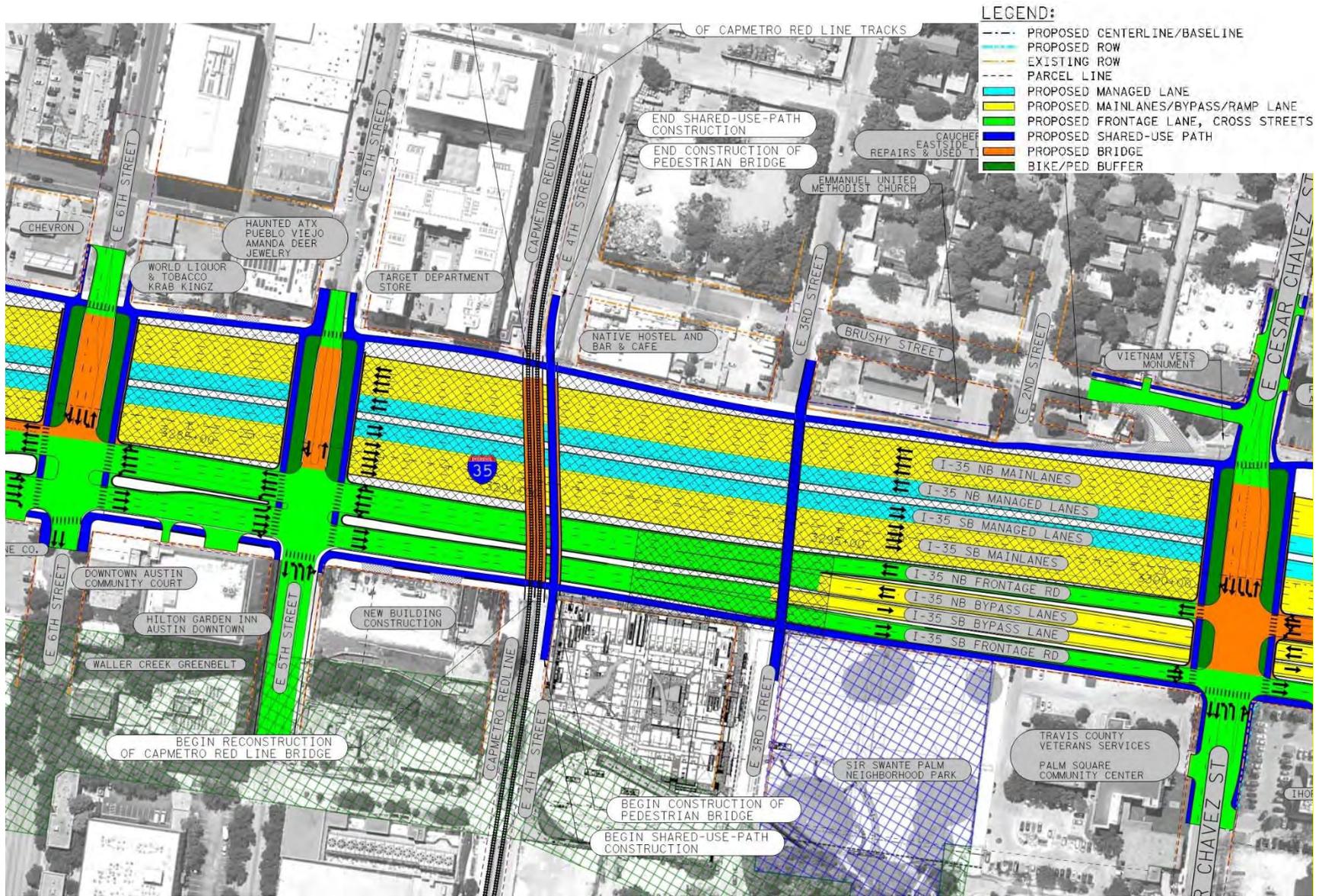
*Figure 24. Cross-section of Alternative 2 project schematic showing proposed configuration in the Walker Brothers Warehouse vicinity.*

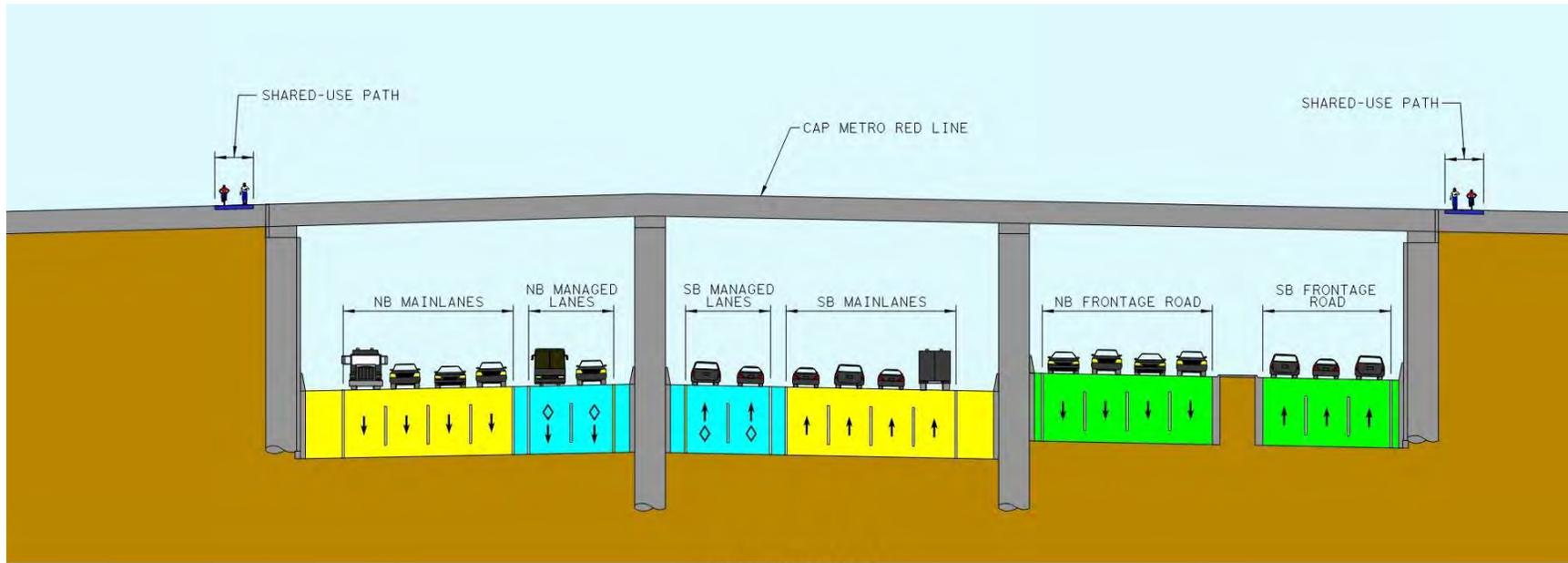
No ROW or easements would be acquired from the Walker Brothers Warehouse property. Based on April 2022 project information for Alternative 2, the at-grade shared-use path would essentially replace and upgrade the existing sidewalk and would directly abut the building face. The non-historic-age metal fencing surrounding the open storage area south of the Walker Brothers Warehouse building may be located in existing I-35 ROW. The fence may be removed to accommodate the shared-use path. However, the southern portion of the property parcel including the fence is not historically associated with the warehouse building and is excluded from the NRHP-eligible boundaries. The Walker Brothers Warehouse building face would be approximately 10 feet from the northbound frontage road pavement edge and 95 feet from the nearest main lane. While proposed northbound main lanes would be approximately 30 feet closer to the Walker Brothers Warehouse compared with the current configuration, the vehicular lanes would be depressed below grade, reducing I-35 noise and visual impacts from the current elevated profile. Vehicular access from East 4<sup>th</sup> Street to the I-35 frontage road would be lost. However, pedestrian/bicycle access at this location would be improved with construction of a dedicated pedestrian bridge.

The Capital Metro Red Line bridge would introduce a new visual obstruction, approximately six feet in height and approximately 50 feet north of the Walker Brothers Warehouse north building face. However, the proposed Capital Metro Red Line bridge would be substantially lower in height than the existing I-35 main lanes. Additional noise impacts from rail traffic on the bridge would be very minor, given the presence of the existing at-grade rail line, elevated I-35 main lanes, and adjacent at-grade I-35 northbound frontage road. Based on the information presented above, Alternative 2 would have **no adverse effect** to Resource 400, the Walker Brothers Warehouse at 807 East 4<sup>th</sup> Street.

### ***Modified Alternative 3***

At this location, I-35 would be reconstructed to include four main lanes and two managed lanes in each direction. Three-lane northbound and southbound at-grade frontage roads would be located on the west side of the freeway. All vehicular lanes would be depressed below grade. Shared-use paths would be located at-grade (see Figures 25 and 26). The Capital Metro Red Line tracks would be carried on a bridge structure over the depressed vehicular lanes. The Capital Metro Red Line will be approximately six feet above existing ground level at the edge of the I-35 ROW and will lower from west to east, matching the existing rail profile just west of San Marcos Street. A separate pedestrian bridge, adjacent to the south side of the railroad bridge, would carry a shared-use path over the depressed vehicular lanes. Vehicular access from East 4<sup>th</sup> Street to northbound I-35 would be lost. However, as noted above, pedestrian/bicycle access across I-35 at East 4<sup>th</sup> Street would be maintained with construction of a pedestrian bridge and shared-use path. The nearest vehicular access to the I-35 northbound frontage road would be provided at East 5<sup>th</sup> Street and East Cesar Chavez Street. There is potential for construction of a concrete cap over the depressed main lanes and managed lanes from East Cesar Chavez Street to East 8<sup>th</sup> Street (see Figures 27 and 28).





**SECTION I-I**

Figure 26. Cross-section of Modified Alternative 3 project schematic showing proposed configuration in the Walker Brothers Warehouse vicinity.



### 3RD STREET - LOOKING NORTHEAST

Figure 27. Visualization of Modified Alternative 3 project schematic (January 22, 2022, version) showing proposed configuration in Walker Brothers Warehouse vicinity, without decks. Building is visible at top center of frame at I-35/East 4<sup>th</sup> Street.



**SURFACE LEVEL ENHANCEMENTS TO BE PROVIDED BY OTHERS**

**3RD ST. WITH DECK PLAZA - LOOKING NORTHEAST**

*Figure 28. Visualizations of Modified Alternative 3 project schematic (January 22, 2022 version) showing proposed configuration in Walker Brothers Warehouse vicinity, with decks. Building is visible at top left of frame at I-35/East 4<sup>th</sup> Street.*

No ROW or easements would be acquired from the Walker Brothers Warehouse property. Based on April 2022 project information for Modified Alternative 3, the at-grade shared-use path would essentially replace and upgrade the existing sidewalk and would directly abut the building face. The non-historic-age metal fencing surrounding the open storage area south of the Walker Brothers Warehouse building may be located in existing I-35 ROW. The fence may be removed to accommodate the shared-use path. However, the southern portion of the property parcel including the fence is not historically associated with the warehouse building and is excluded from the NRHP-eligible boundaries. The Walker Brothers Warehouse building face would be approximately 27 feet from the retaining wall for the depressed freeway section and approximately 37 feet from the nearest main lane. While proposed northbound main lanes would be considerably closer to the Walker Brothers Warehouse compared with the current configuration, the vehicular lanes would be depressed below grade, in contrast to the current above-grade main lanes and at-grade frontage roads. The depressed freeway configuration would reduce I-35 noise and visual impacts from the current elevated profile. Noise and visual impacts would be further reduced if the depressed lanes are covered with the potential concrete cap. Vehicular access from East 4<sup>th</sup> Street to the I-35 frontage road would be lost. However, pedestrian/bicycle access at this location would be improved with construction of a dedicated pedestrian bridge.

The Capital Metro Red Line bridge would introduce a new visual obstruction, approximately six feet in height and approximately 50 feet north of the Walker Brothers Warehouse north building face. However, the proposed Capital Metro Red Line bridge would be substantially lower in height than the existing I-35 main lanes. Additional noise impacts from rail traffic on the bridge would be very minor, given the presence of the existing at-grade rail line, elevated I-35 main lanes, and adjacent at-grade I-35 northbound frontage road. Based on the information presented above, Alternative 2 would have **no adverse effect** to Resource 400, the Walker Brothers Warehouse at 807 East 4<sup>th</sup> Street.

- **Indirect, Cumulative or Reasonable Foreseeable Effects**

Large-scale residential and commercial development, along with increased traffic on I-35 and city streets, are existing developmental trends already present in the immediate Walker Brothers Warehouse vicinity. The proposed project does not deviate from established urban development trends that result in both beneficial and adverse impacts to historic resources from large infrastructure projects; these trends are not likely to be substantially changed by this project. A cumulative impacts analysis, including cumulative effects to historic properties, is being prepared by TxDOT as a separate technical report for the overall I-35 Capital Express Central Project. The proposed project would add traffic capacity to I-35 and may alter specific traffic patterns or access to the Walker Brothers Warehouse. However, the project is not expected to result in indirect, cumulative, or reasonably foreseeable effects on the Walker Brothers Warehouse.

## **U.S. DOT Section 4(f) Applicability Statement**

In the intensive survey area, the proposed project would not result in a use of any Section 4(f)-protected properties for transportation purposes.

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## **Appendix A: Project Information and ROW Information**

- [WPD Section I - Project Definition](#)
- [WPD Section II - Tool](#)
- [WPD Section III - Project Work Plan](#)
- [WPD Section IV - Findings](#)



[Archived WPD I](#)

Project Definition

Project Name:

CSJ:  -  -

Anticipated Environmental Classification:

Is this an FHWA project that normally requires an EIS per 23 CFR 771.115(a)?

Project Association(s)

Manually Associate CSJ:

CSJ	DCIS Funding	DCIS Number	Env Classification	DCIS Classification	Main or Associate	Doc Tracked In	Actions
<a href="#">CSJ:091404341</a>			EIS		Associate	Main	
<a href="#">CSJ:001513433</a>			EIS		Associate	Main	
<a href="#">CSJ:001513432</a>			EIS		Associate	Main	
<a href="#">CSJ:001513423</a>	Federal,State	F ( )	EIS	WF	Associate	Main	
<a href="#">CSJ:001513428</a>	Federal,State	F ( )	EIS	FOI	Associate	Main	
<a href="#">CSJ:001513399</a>	State	ROW 15-13-399	EIS	ROW	Associate	Main	

DCIS Project Funding and Location

Funding

DCIS Funding Type:

Federal  State  Local  Private

Location

DCIS Project Number:  Highway:

District:  County:

Project Limit -- From:

Project Limit -- To:

Begin Latitude: +  .  Begin Longitude: -  .

End Latitude: +  .  End Longitude: -  .

DCIS & P6 Letting Dates

DCIS District:  DCIS Approved:  DCIS Actual:

P6 Ready To Let:  P6 Proposed Letting:

DCIS Project Description

Type of Work:

Layman's Description:

DCIS Project Classification:

Design Standard: 4R - New Location and Reconstruction

Roadway Functional Classification: 1 - Interstate

Jurisdiction

No

Does the project cross a state boundary, or require a new Presidential Permit or modification of an existing Presidential Permit?

Who is the lead agency responsible for the approval of the entire project?

FHWA - Assigned to TxDOT  TxDOT - No Federal Funding  FHWA - Not Assigned to TxDOT

TxDOT

Who is the project sponsor as defined by 43 TAC 2.7?

No

Is a local government's or a private developer's own staff or consultant preparing the CE documentation, EA or EIS?

Yes

Does the project require any federal permit, license, or approval?

USACE  IBWC  USCG  NPS  IAJR  Other Formal USFWS consultation is anticipated, th

No

Does the project occur, in part or in total, on federal or tribal lands?

Environmental Clearance Project Description

Project Area

Typical Depth of Impacts: 26 (Feet)

Maximum Depth of Impacts: 80 (Feet)

New ROW Required: 44 (Acres)

New Perm. Easement Required: 0 (Acres) New Temp. Easement Required: 3.9 (Acres)

Project Description

Describe Limits of All Activities: Spell

TxDOT is proposing improvements to I-35 from US290E to US290W/SH71 in Travis County (~8 miles in length).

The proposed improvements include the removal of the existing I-35 decks from Airport Blvd. to MLK Jr. Blvd., lowering the roadway, and adding two HOV managed lanes in each direction. One alternative would add direct connectors at I-35/US 290E. The project will also reconstruct east-west cross-street bridges, add shared-use paths (SUP), and make additional safety and mobility improvements within the project limits.

TxDOT, in coordination with the City of Austin and the University of Texas (UT), is designing the project to accommodate potential deck plaza locations that would cover sections of the main and HOV lanes of I-35 and provide community enhancement opportunities in those areas. The City is currently evaluating potential deck plazas between Cesar Chavez St. and 8th St. UT is evaluating potential deck cap areas between Dean Keeton St. and 15th St. In addition, "stiches," or bridges with enhanced (widened) pedestrian and bicycle accommodations and amenities, are being evaluated at the following locations: the CapMetro Red Line crossing south of Airport Blvd., Wilshire Blvd., 38th 1/2 St., 32nd St., 12th St., 11th St., Holly St., and Woodland Ave.

In addition, this project will make improvements to the drainage system including potential drainage tunnels and outfall sites. Currently, TxDOT is proposing major drainage systems along and

Describe Project Setting:

The proposed project location is in an urban setting. The existing roadway experiences high traffic volume throughout the day, as I-35 is one of only three north-south-oriented controlled-access facilities in the Austin metropolitan area.

Land use in the vicinity of the project area is highly developed and comprised of a variety of property types including commercial (large shopping and office/retail centers, car dealerships, hotels/motels, restaurants, municipal buildings), churches, hospitals/health care providers, schools, parks, and residential (single-family residential and multi-family apartment and condominium complexes). There are a few undeveloped parcels; however, none are being used for cropland, pasture, or range land.

Eight stream features cross the project area and include two tributaries of Tannehill Branch (intermittent), Boggy Creek (intermittent flow regime), Lady Bird Lake (perennial), two unnamed tributaries of the Colorado River/Lady Bird Lake (one perennial and one ephemeral), and two tributaries of Blunn Creek (one ephemeral and one intermittent). Vegetation in the project area consists of maintained roadside grasses and forbs within existing ROW. Landscaped grasses, forbs, and shrubs are located within developed areas. In undeveloped areas, vegetation consists of disturbed pasture, Ashe juniper/live oak woodlands, and narrow riparian areas.

Cemeteries adjacent to the project area include Mt. Calvary, Oakwood, and Assumption.

There are Section 4(f)- and Section 6(f)-protected parkland properties adjacent to the project limits, including: Northwest Greenway along Philomena Street; Swede Hill Pocket Park; Waller Creek Greenbelt; Waterloo Greenway; Waller Beach at Town Lake Metro Park (Section 6(f)); Sir Swante Palm Neighborhood Park; Edward Rendon Sr. Metro Park at Festival Beach (Section 6(f)); Norwood Tract at Town Lake Metro Park; and Ann and Roy Butler Hike and Bike 1300 Riverside Easement. There are also some historic sites that may trigger individual Section 4(f) evaluations, such as the Haster House (Glass Coffin), Austin Chronicle Building and the Dura Tune Service Station.

There are adjacent properties that are listed in or are eligible for the NRHP (i.e. Mt. Calvary Cemetery, Austin Chronicle building, Palm Park, etc.). This project is located in USFWS karst zone 3B, but it is not located within the Edwards Aquifer Recharge Zone.

There is potential freshwater mussel habitat in the Colorado River near Longhorn Dam.

#### Describe Existing Facility:

The existing I-35 roadway from US290E to US 290W/SH71 is located in an urban area with adjacent commercial, residential, institutional, governmental, and parks/open space properties. Within the proposed project limits, I-35 is an access-controlled interstate highway. Beginning at the southern limit, US 290W/SH 71, the roadway typically has three to four, 12-foot-wide mainlanes (concrete barrier-separated) with 4- to 12-foot-wide inside shoulders, 10- or 12-foot-wide outside shoulders, and two to three, 11- or 12-foot-wide frontage road lanes with curb and gutter in each direction. From Lady Bird Lake to 15th St., I-35 generally includes three 12-foot-wide mainlanes in each direction with auxiliary lanes between some of the ramps. North of 15th St., the roadway has four mainlanes in each direction and includes the upper/lower deck split just north of MLK Jr. Blvd. with a continuation of the upper decks to north of Airport Blvd. From Airport Blvd. to US 290E, I-35 includes four barrier-separated mainlanes in each direction. The roadway here typically has 2- to 6-foot-wide inside shoulders, 10-foot-wide outside shoulders, and two to four, 11- or 12-foot-wide frontage road lanes with curb and gutter in each direction. US 290E, between I-35 and Cameron Rd., is a four-lane freeway with 12-foot-wide mainlanes in each direction and 6-foot inside and 5'-20' outside shoulders. Frontage roads are 2 to 4 lanes in each direction and direct connector ramps provide access to and from the I-35 general purpose lanes.

Sidewalks exist in most, but not all, locations throughout the project area and SUP are located within the project area in "downtown" Austin, defined as between MLK Jr. Blvd. and Holly St. Drainage along the roadway (mainlanes and frontage roads) is provided by storm sewer networks and

#### Describe Proposed Facility:

~8 m in length - 1,500 ft north of US290E to 1,000 ft south of SH71. Removal of decks (Airport to MLK), lowering roadway, adding 2 HOV lanes in each direction, reconstructing E-W cross-streets, adding SUP.

Construction access/staging needed within parkland: Waller Beach (6(f)); Edward Rendon (6(f)); Norwood Tract; & Ann/Roy Butler Hike & Bike Easement.

Improvements to drainage include new drainage systems & outfalls. Project lowers roadway profile below existing grade north/south of Lady Bird Lake, which severs drainage systems connected to Harpers Branch, Lady Bird Lake, Colorado River, Waller Creek, & Boggy Creek. New systems needed to drain runoff severed from existing outfalls: storm drains along both FRs & MLs (Harper's Branch outfall to Oltorf); tunnel east I35 (Lady Bird Lake to 15th); tunnel west I35 (Waller Creek outfalls [3rd, 9th, & 15th] to Hancock Center), tunnel down Cesar Chavez (CO River downstream of Longhorn Dam to I35); tunnel Clarkson Branch to I35 (via 38th $\frac{1}{2}$  & north to Hancock Center); & storm drain Boggy Creek to the west of I35 (via a crossing north of Airport).

Based on alternatives screening process, TxDOT will analyze 2 build alts. and the No Build in the EIS:

Alt. 2

Would the project add capacity?

Transportation Planning

Is the project within an MPO's boundaries?

Does the project meet the definition for a grouped category for planning and programming purposes?

The project is located in  area.

This status applies to:

CO - Carbon Monoxide

O3 - Ozone

NO2 - Nitrogen Dioxide

PM10 - Particulate

PM2.5 - Particulate

Environmental Clearance Information

Environmental Clearance Date:

Environmental LOA Date:

Closed Date:

Archived Date:

Approved Environmental Classification:

Project Contacts

Created By:

Date Created:

Project Sponsor:  TXDOT (Or)  Local Government

Sponsor Point Of Contact:

ENV Core Team Member:

District Core Team Member:

Other Point of Contact(s):

Last Updated By: Tricia Bruck-Hoyt-C

Last Updated Date: 04/08/2022 11:13:40

## **Appendix B: Tabular Inventory of Surveyed Properties**

Resource No.	Address/ Location	Function/ Sub-function	Architectural Style	Date(s)	Description/Comments	Integrity Considerations	NRHP Eligibility
400	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495	INDUSTRY/ warehouse	Commercial	c.1880	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>	<p>The replacement of windows and doors on the original building, and the infilling of warehouse bays on the additions, was sensitive to the historic fenestration and does not diminish the warehouse's integrity of materials and workmanship. Modern development surrounding the warehouse has affected its integrity of setting. However, the exterior of the building is largely intact and therefore its integrity of location, design, feeling, and association are retained. Its integrity is sufficient to convey its significance under NRHP Criterion A as a former industrial property and under NRHP Criterion C for its architecture.</p>	Eligible (Criterion A: Industry; Criterion C: Architecture)

## **Appendix C: Survey Forms for All Surveyed Properties**

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
Integrity/Comments:	<p>The replacement of windows and doors on the original building, and the infilling of warehouse bays on the additions, was sensitive to the historic fenestration and does not diminish the warehouse’s integrity of materials and workmanship. Modern development surrounding the warehouse has affected its integrity of setting. However, the exterior of the building is largely intact and therefore its integrity of location, design, feeling, and association are retained. Its integrity is sufficient to convey its significance under NRHP Criterion A as a former industrial property and under NRHP Criterion C for its architecture.</p>



Overall view, facing southwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
Integrity/Comments:	<p>The replacement of windows and doors on the original building, and the infilling of warehouse bays on the additions, was sensitive to the historic fenestration and does not diminish the warehouse's integrity of materials and workmanship. Modern development surrounding the warehouse has affected its integrity of setting. However, the exterior of the building is largely intact and therefore its integrity of location, design, feeling, and association are retained. Its integrity is sufficient to convey its significance under NRHP Criterion A as a former industrial property and under NRHP Criterion C for its architecture.</p>



Side (east) elevation of the original c.1880 building, view facing southwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
Integrity/Comments:	<p>The replacement of windows and doors on the original building, and the infilling of warehouse bays on the additions, was sensitive to the historic fenestration and does not diminish the warehouse's integrity of materials and workmanship. Modern development surrounding the warehouse has affected its integrity of setting. However, the exterior of the building is largely intact and therefore its integrity of location, design, feeling, and association are retained. Its integrity is sufficient to convey its significance under NRHP Criterion A as a former industrial property and under NRHP Criterion C for its architecture.</p>



Front (north) facade, view facing south.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
Integrity/Comments:	<p>The replacement of windows and doors on the original building, and the infilling of warehouse bays on the additions, was sensitive to the historic fenestration and does not diminish the warehouse’s integrity of materials and workmanship. Modern development surrounding the warehouse has affected its integrity of setting. However, the exterior of the building is largely intact and therefore its integrity of location, design, feeling, and association are retained. Its integrity is sufficient to convey its significance under NRHP Criterion A as a former industrial property and under NRHP Criterion C for its architecture.</p>



Detail of second-story windows on facade, view facing southwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
Integrity/Comments:	<p>The replacement of windows and doors on the original building, and the infilling of warehouse bays on the additions, was sensitive to the historic fenestration and does not diminish the warehouse’s integrity of materials and workmanship. Modern development surrounding the warehouse has affected its integrity of setting. However, the exterior of the building is largely intact and therefore its integrity of location, design, feeling, and association are retained. Its integrity is sufficient to convey its significance under NRHP Criterion A as a former industrial property and under NRHP Criterion C for its architecture.</p>



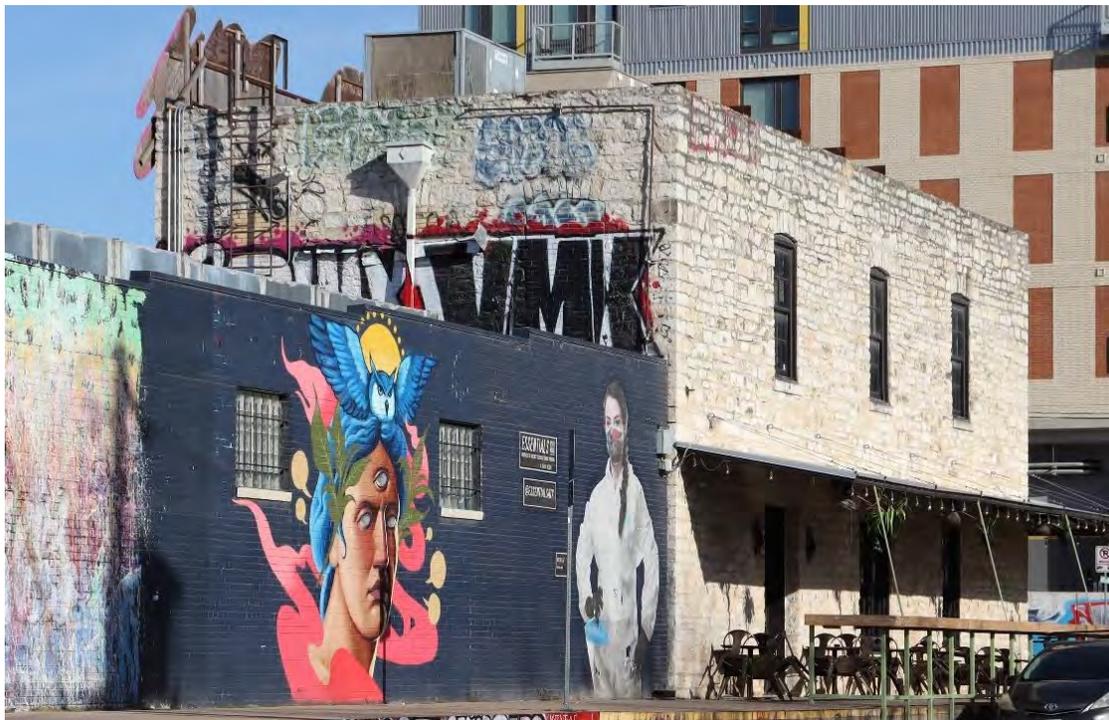
Detail of entry door on facade, view facing southwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
Integrity/Comments:	<p>The replacement of windows and doors on the original building, and the infilling of warehouse bays on the additions, was sensitive to the historic fenestration and does not diminish the warehouse’s integrity of materials and workmanship. Modern development surrounding the warehouse has affected its integrity of setting. However, the exterior of the building is largely intact and therefore its integrity of location, design, feeling, and association are retained. Its integrity is sufficient to convey its significance under NRHP Criterion A as a former industrial property and under NRHP Criterion C for its architecture.</p>



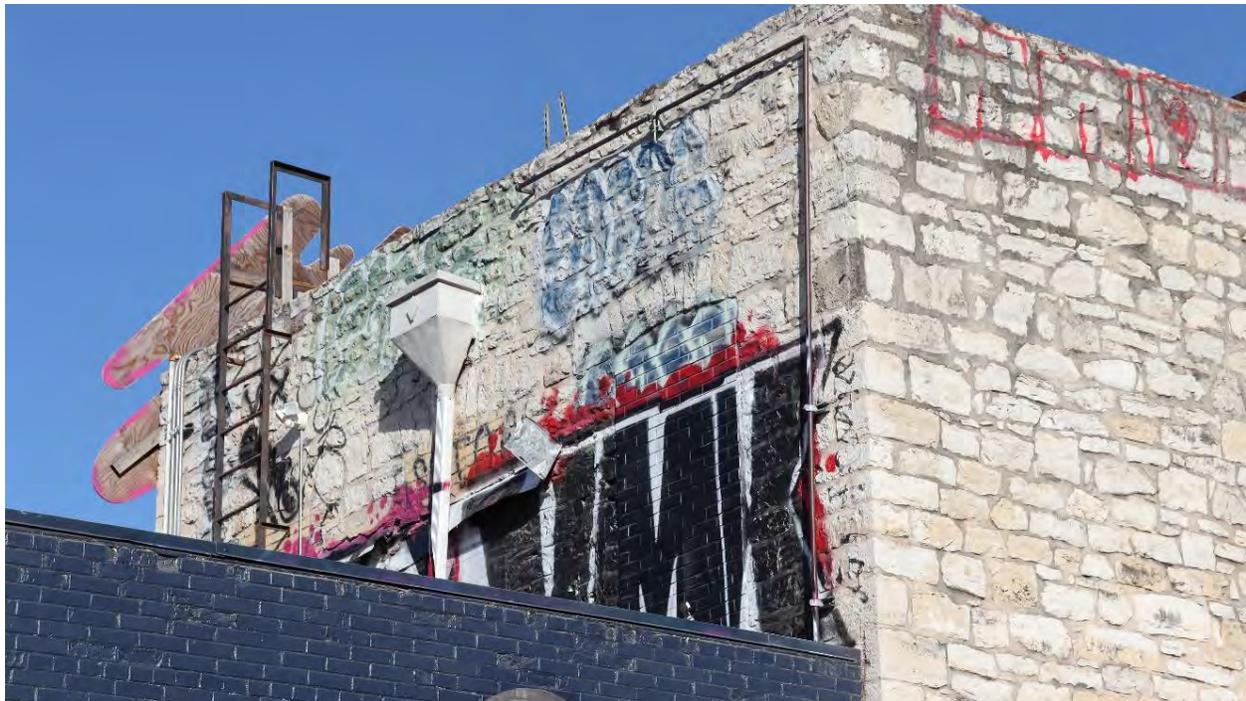
Side (west) elevation of the original c.1880 building, view facing southeast.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
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Rear (south) and side (east) elevation of the original c.1880 building, view facing northwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
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Detail of second-story rear (south) elevation of the original c.1880 building, view facing northwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
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Overall view of the c.1935 and c.1953 additions, view facing southwest.

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Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
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Front (north) facade of the c.1935 addition, view facing south.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
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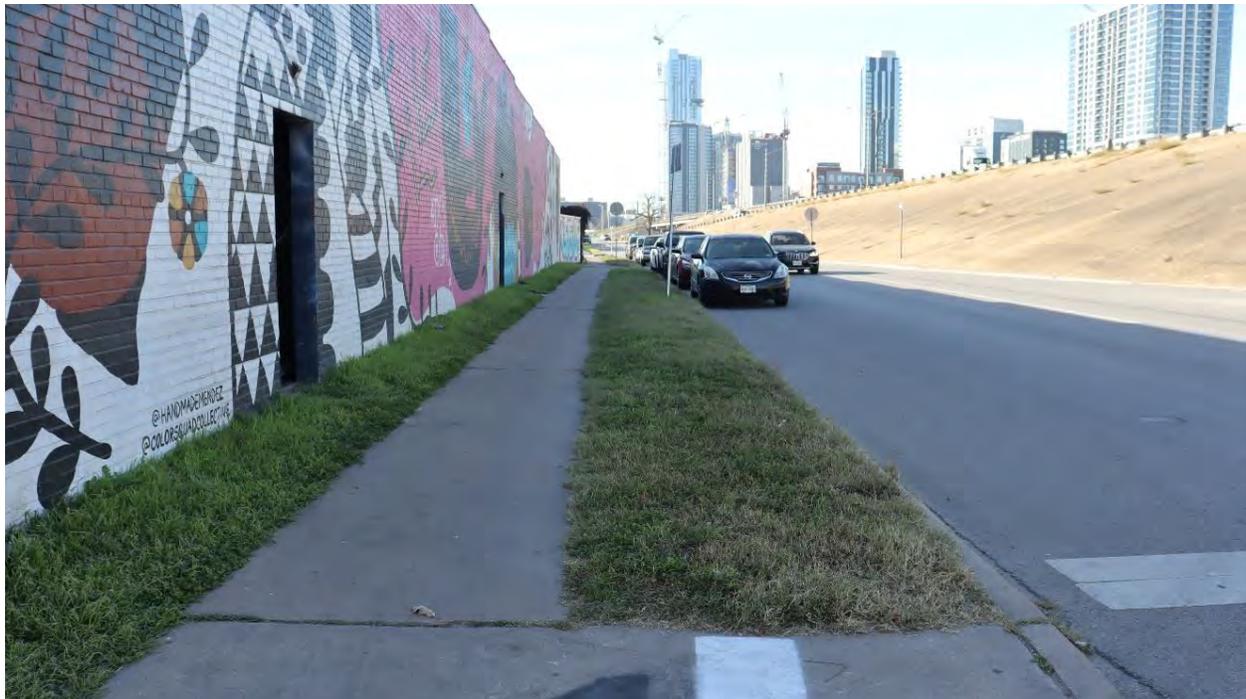
Detail of entry on facade of the c.1935 addition, view facing southeast.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
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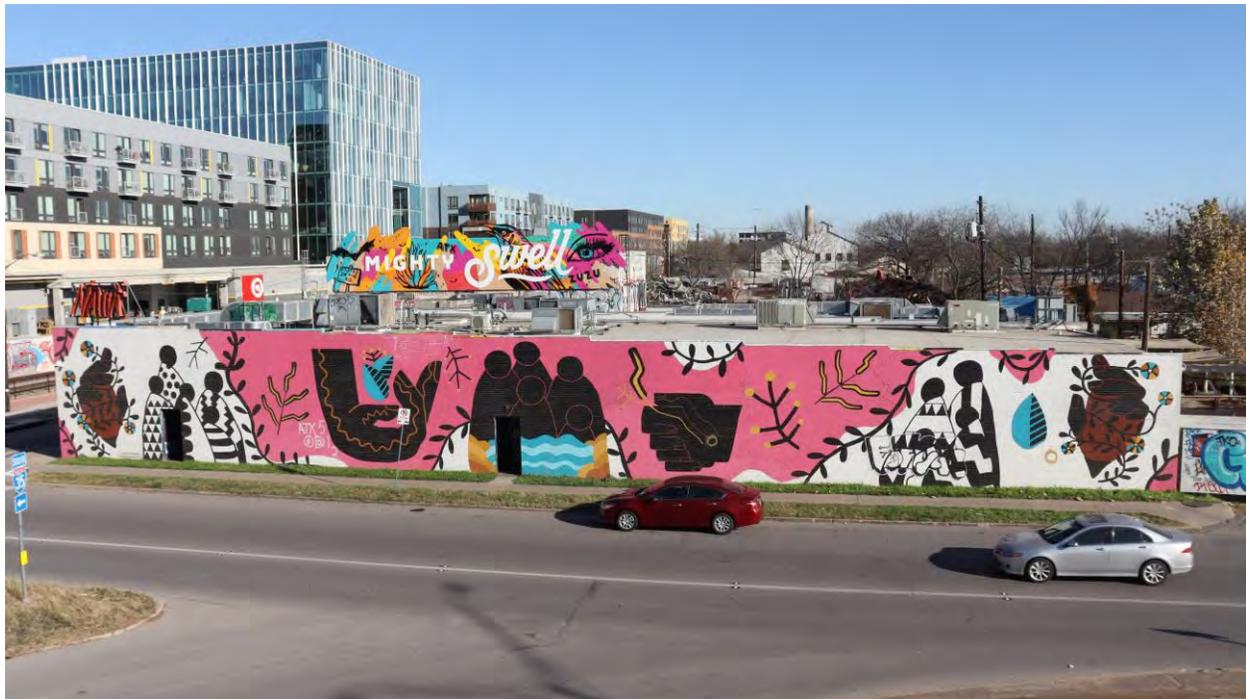
Front (north) facade of the c.1953 addition, view facing south.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
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Side (west) elevation of the c.1953 addition, I-35 northbound frontage road, and I-35, view facing south.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
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Side (west) elevations, view facing east.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
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Construction Date:	c.1880
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Detail of entry door on side (west) elevation of the c.1953 addition, view facing southwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
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Overview of the property from I-35, view facing northeast.

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Rear (south) elevation, courtyard, parking, and ad-hoc storage, view facing north.

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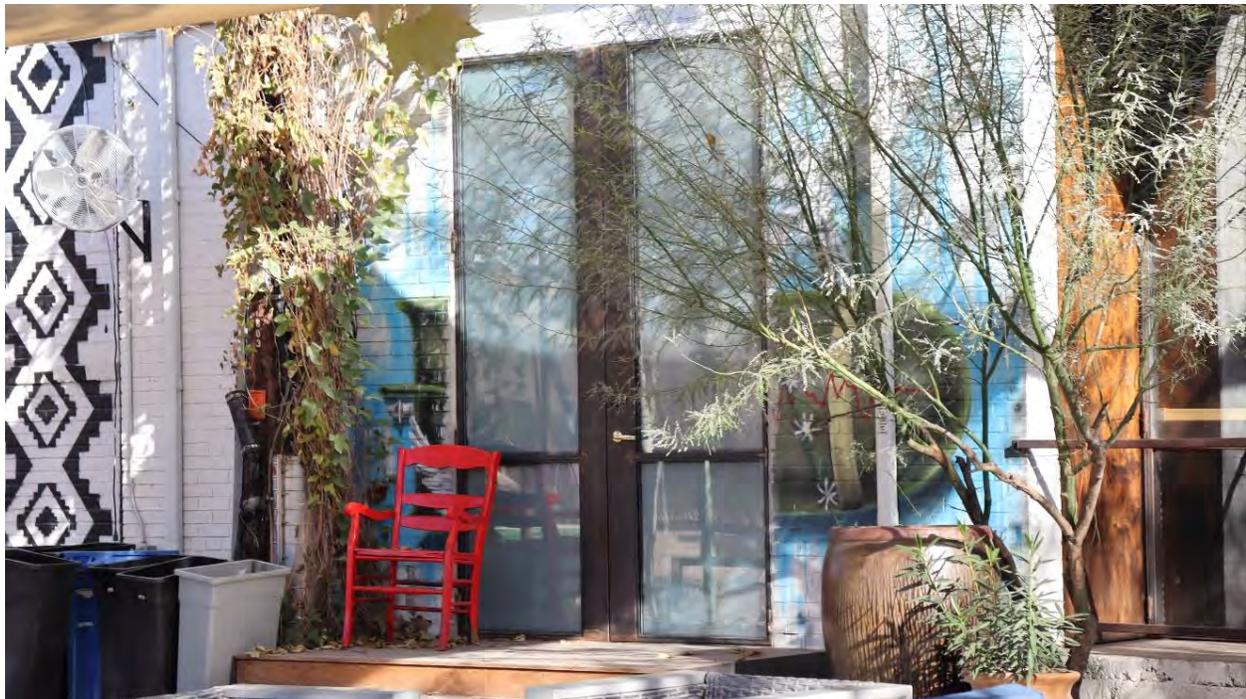
Rear (south) elevation of the c.1935 and c.1953 additions and the courtyard, view facing northwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
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Rear (south) elevation of the c.1955 addition, view facing northwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
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NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
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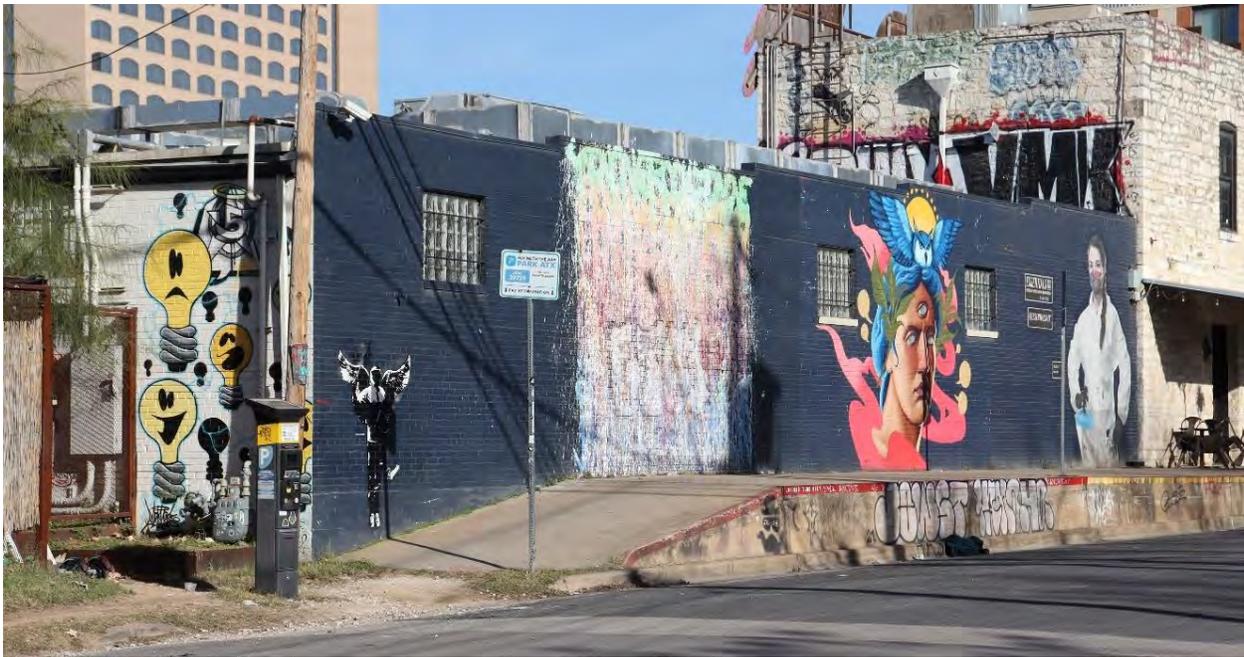
Detail of entry on rear (south) elevation of the c.1935 addition, view facing northwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
Function/Sub-function:	INDUSTRY/warehouse
Construction Date:	c.1880
NRHP Eligibility:	<p>The Walker Brothers Warehouse was built c.1880. It is a two-story commercial building of rubble limestone construction with visible tooling marks. The rectangular-plan structure has a flat, membranous roof. Fenestration generally consists of replacement, four-over-four-unit, wood sash and metal fixed windows with heavy glazed metal replacement doors; decorative limestone sills and segmental arch lintels frame these openings. Flat, corrugated metal awnings are suspended from metal anchors along the front and side (east) elevation. Two historic-age additions replace the first story of the side (west) elevation of the original two-story stone building. Landscaping consists of metal planters placed on the sidewalk along the front (north) facade and grass flanking the sidewalk along the side (west) elevation of the building.</p> <p>Resource 400 is significant under NRHP Criterion A in the area of Industry and NRHP Criterion C in the area of Architecture. It does not possess historical or architectural significance within the historic contexts necessary for NRHP eligibility under Criterion B.</p>
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Detail of entry on rear (south) elevation of the c.1935 addition, view facing northwest.

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Side (east) elevation of the c.1935 addition, view facing northwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
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Side (east) elevation of the c.1935 addition, view facing southwest.

Survey Date:	January 14, 2022
Resource No:	400
Project Location:	Austin, Travis County, I-35: US 290 East to US 290 West/SH 71
Project Name and CSJ:	I-35 Capital Express Central – Walker Brothers Warehouse Intensive Survey; 0015-13-388
Address, Lat/Long:	807 East 4 <sup>th</sup> Street, Austin, Texas 78702; 30.319018, -97.706495
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Detail of brick and window on the side (east) elevation of the c.1935 addition, view facing west.

## Appendix D: Figures



**NRHP Boundary Map**  
**I-35 Capital Express Central**  
**Intensive Survey**  
**CSJ: 0015-13-388**  
**April 2022**

- APE
- NRHP Boundary
- Parcel Boundary (Travis CAD)
- Existing ROW (TxDOT)

\*No New ROW is Proposed in this Location

- NRHP Eligibility**
- Eligible

*Resource Name: Walker Brothers Warehouse*  
*Resource Number: 400*

